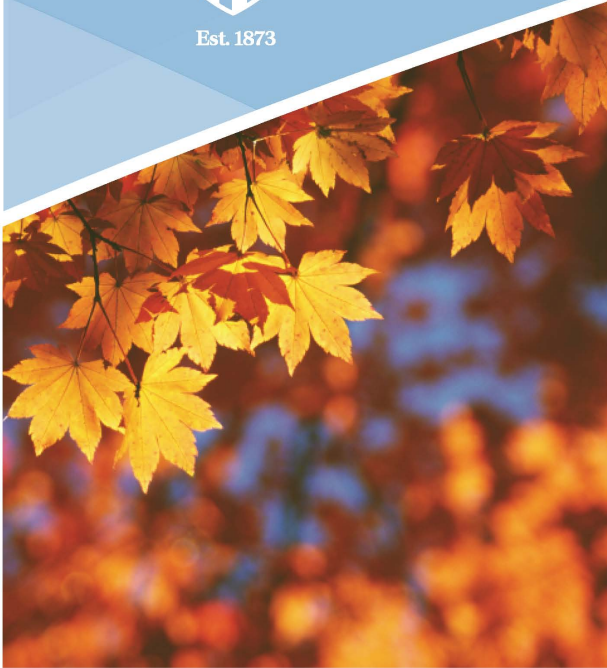


Township of  
Langley



Est. 1873



# Aldergrove

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## Community Plan



Adopted January 8, 1979



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# THE CORPORATION OF THE TOWNSHIP OF LANGLEY

## ALDERGROVE COMMUNITY PLAN BYLAW 1978 NO. 1802

Adopted by Council on January 8, 1979

### **CONSOLIDATED FOR CONVENIENCE ONLY**

THIS IS A CONSOLIDATED BYLAW PREPARED BY THE CORPORATION OF THE TOWNSHIP OF LANGLEY FOR CONVENIENCE ONLY. THE TOWNSHIP DOES NOT WARRANT THAT THE INFORMATION CONTAINED IN THIS CONSOLIDATION IS CURRENT. IT IS THE RESPONSIBILITY OF THE PERSON USING THIS CONSOLIDATION TO ENSURE THAT IT ACCURATELY REFLECTS CURRENT BYLAW PROVISIONS.

### AMENDMENTS

<u>BYLAW NO. / NAME</u>	<u>MAP/TEXT CHANGE</u>	<u>DATE OF ADOPTION</u>
2439 (Eze Rent It)	Map	August 18, 1986
2753 (Kinsmen)	Map/Text	October 2, 1989
2790 (Azalea Holdings)	Map/Text	November 5, 1990
3153 (Charlton)	Map	September 14, 1992
3289 (Township of Langley)	Map/Text	October 4, 1993
3462 (Development Permit Areas)	Map/Text	January 23, 1995
3609 (Roanoak)	Map/Text	August 26, 1996
3711 (Development Permit)	Text	July 7, 1997
3675 (Azalea)	Text	July 28, 1997
3772 (Progressive Properties Ltd.)	Map	December 21, 1998
3759 (Aldergrove Alliance Church)	Map	May 3, 1999
3940 (Super L'Auto Recyclers Ltd.)	Map/Text	December 11, 2000
3898 (Aldergrove Centre Mall)	Map/Text	December 3, 2001
4021 (Robinson)	Map	April 8, 2002
4083 (Dallas Developments Ltd.)	Map	July 22, 2002
3960 (Kohler/595469 BC Ltd.)	Map	September 9, 2002
4087 (Schmidt)	Map	November 18, 2002
4205 (HR Pacific Construction Mgmt Ltd)	Map/Text	January 12, 2004
4182 (Aldergrove Seniors' Care Facility)	Map/Text	March 22, 2004
4326 (HR Pacific Construction Mgmt Ltd)	Map	February 21, 2005
4241 (Anderson/Ott)	Map	August 29, 2005
4081 (Fox and Hounds)	Map	December 19, 2005
4464 (Orwick/Jones)	Map	January 22, 2007
4824 (Aldergrove Core Area Plan)	Map/Text	September 13, 2010
5041 (Township of Langley/Sanderson)	Map	April 28, 2014
5076 (HY Eng. Ltd./ AP Tire Services Ltd.)	Map/Text	June 15, 2015
5126 (Noort Developments Ltd.)	Map	March 21, 2016

## Table of Contents

<b>PART 1 – PLAN BASIS .....</b>	<b>5</b>
1.0 INTRODUCTION.....	5
2.0 ENABLING LEGISLATION.....	5
3.0 DESCRIPTION OF THE PLAN.....	5
4.0 PURPOSE OF THE PLAN.....	5
5.0 FUTURE PROVISIONS.....	5
6.0 PLAN COMPONENTS .....	5
7.0 GOALS AND OBJECTIVES.....	6
7.1 LAND USE PATTERN .....	6
7.2 ECONOMIC.....	7
7.3 ENVIRONMENTAL .....	7
<b>PART II – PLAN DOCUMENT .....</b>	<b>8</b>
1.0 LAND USE DESIGNATIONS .....	8
2.0 AREA BOUNDARIES .....	8
3.0 INTERPRETATION .....	8
4.0 GENERAL PROVISIONS .....	9
4.1 ENVIRONMENTAL PROTECTION .....	9
4.2 DEVELOPMENT PERMITS .....	9
4.3 SPECIAL CONDITIONS .....	11
4.4 REGULATORY BYLAWS.....	12
<b>PART III – LAND USE POLICIES.....</b>	<b>13</b>
1.0 RESIDENTIAL .....	13
2.0 COMMERCIAL AREAS .....	14
2.5 TOWN CENTRE .....	15
3.0 INDUSTRIAL AREAS.....	15
4.0 RECREATION AND EDUCATION AREAS.....	16
5.0 CONSERVATION AREAS .....	17
<b>PART IV – SERVICING POLICIES.....</b>	<b>19</b>
1.0 PURPOSE.....	19
1.1 WATER SUPPLY .....	19
1.2 SANITARY SEWER SERVICES .....	19
1.3 STORM SEWER SERVICES.....	19
1.4 TRANSPORTATION .....	20
<b>PART V – IMPLEMENTATION OF THE PLAN .....</b>	<b>22</b>
1.0 GENERAL .....	22
2.0 DEVELOPMENT STAGING .....	22
3.0 CAPITAL PROGRAMMING .....	22

4.0 MUNICIPAL REGULATIONS .....	22
<b>PART VI – ALDERGROVE CORE AREA PLAN.....</b>	<b>23</b>
1.0 INTRODUCTION .....	23
1.1 PURPOSE.....	23
1.2 PLANNING CONTEXT.....	23
1.3 PLANNING PROCESS .....	24
1.4 PLAN AREA OVERVIEW .....	25
1.5 PLANNING GOALS .....	27
1.6 PLAN OVERVIEW .....	28
2.0 ALDERGROVE CORE AREA LAND USE PLAN .....	30
2.1 LAND USE PLAN .....	30
ALDERGROVE CORE AREA LAND USE PLAN .....	30
2.2 LAND USE DESIGNATIONS .....	31
3.0 PLAN POLICIES.....	32
3.1 LAND USE AND DEVELOPMENT .....	32
3.2 ACCESS AND MOBILITY.....	33
3.3 HOUSING.....	38
3.4 PUBLIC OPEN SPACE, HERITAGE, COMMUNITY AND CULTURE.....	39
4.0 IMPLEMENTATION.....	43
4.1 INCENTIVES .....	43
4.2 SERVICING AND INFRASTRUCTURE .....	44
4.3 HABITAT STEWARDSHIP .....	44
4.4 DEVELOPMENT PERMIT AREA.....	44
4.5 PREVALANCE OF THE ALDERGROVE CORE AREA PLAN .....	44
<b>APPENDIX A.....</b>	<b>47</b>
ALDERGROVE CORE AREA CONCEPT PLAN AND DEVELOPMENT PERMIT AREA DESIGN GUIDELINES .....	47
1.0 INTRODUCTION.....	50
1.1 DESIGNATION AND JUSTIFICATION .....	50
1.2 PURPOSE AND INTENT .....	50
2.0 THE CONCEPT PLAN.....	52
2.1 OVERVIEW.....	52
2.2 COMMERCIAL CONTINUITY: ACTIVE RETAIL FRONTAGE.....	53
2.3 RESIDENTIAL STREETS .....	54
2.4 OFF-STREET GREENWAYS .....	55
2.5 DOWNTOWN PARKS AND PLAZAS .....	56
2.6 BERTRAND MEWS .....	57
2.7 MASSING MODEL: BUILD OUT CAPACITY – TO 2041 AND BEYOND.....	58
3.0 DESIGN GUIDELINES .....	60
3.1 INTENT AND OBJECTIVES .....	60

3.2 CONNECTIVITY: ENHANCING THE PEDESTRIAN NETWORK .....	62
3.3 STREET DEFINITION .....	63
3.4 ACTIVE FRONTAGES: TRANSPARENCY .....	64
3.5 PARKING, SERVICING AND ACCESS.....	67
3.6 SCALE AND MASSING .....	69
3.7 WEATHER PROTECTION.....	70
3.8 SIGNAGE AND LIGHTING .....	72
3.9 ARCHITECTURAL CONCEPT: ACHIEVING A HUMAN SCALE.....	74
3.10 GREEN AND HEALTHY BUILDINGS AND LANDSCAPES.....	79
3.11 PUBLIC REALM DESIGN .....	81
3.12 PERSONAL SAFETY, SECURITY AND ACCESSIBILITY .....	83
3.13 TALL BUILDINGS.....	84
3.14 MASTER PLANNING FOR TALL BUILDINGS AND LARGE SITES .....	86

**MAPS**

ALDERGROVE CORE AREA LAND USE PLAN	30
ALDERGROVE LAND USE CONCEPT PLAN	45
ALDERGROVE DEVELOPMENT PERMIT AREAS	46

# THE CORPORATION OF THE TOWNSHIP OF LANGLEY

## BYLAW NO. 1802

### ALDERGROVE COMMUNITY PLAN

#### Part 1 – Plan Basis

#### 1.0 INTRODUCTION

This plan shall be cited as the "ALDERGROVE COMMUNITY PLAN."

#### 2.0 ENABLING LEGISLATION

The Aldergrove Community Plan is prepared in accordance with the requirements of the Local Government Act.

#### 3.0 DESCRIPTION OF THE PLAN

This Community Plan is a declaration of the goals, objectives and policies which will guide the future development of the Aldergrove Urban Area. This Community Plan consists of a written text and an accompanying map schedule designated "Schedule A" titled "ALDERGROVE COMMUNITY PLAN."

#### 4.0 PURPOSE OF THE PLAN

The purpose of this Community Plan is to provide:

- (a) for long range orderly development in accordance with stated objectives;
- (b) a guide for day-to-day decision-making in the development process of the area;
- (c) the basis for the preparation and adoption of the capital works program based on need; and
- (d) the basis for the preparation and adoption of land use regulating bylaws and amendments to them.

#### 5.0 FUTURE PROVISIONS

This plan shall be subject to periodic review in order that it will remain current with major changes that may significantly affect land use, municipal servicing, and the pattern of living.

#### 6.0 PLAN COMPONENTS

The Aldergrove Community Plan shall provide for the allocation of residential uses, commercial uses, industrial uses, recreation, education, and conservation areas, as well as a vehicular and pedestrian circulation system.

An Aldergrove Core Area Plan shall be prepared and adopted by Council to provide:

- a more detailed land use plan;
- policies for provision of water, sewer, storm drainage and transportation servicing;
- policies for provision of school, park, greenway facilities;
- environmental protection policies; and
- implementation policies, including guidelines for development permit areas and prerequisites for provision of services or facilities before development can begin.

The boundaries of the Aldergrove Core Area Plan are shown in Schedule “A” of the Aldergrove Community Plan and are further defined in Part VI – Aldergrove Core Area Plan.

## **7.0 GOALS AND OBJECTIVES**

The goals and objectives in this plan convey the general intent and purpose of the policies and standards stated in this plan.

### **7.1 LAND USE PATTERN**

#### ***Goal***

The orderly development of a variety of land use types each compatible and complimentary to the other and which will promote efficient transportation and servicing systems.

#### ***Objectives - Community Format***

- (a) That the Aldergrove area become a Municipal Town Centre providing for a balanced self-contained community.
- (b) That the basis for development be a mix of density and use, with high density mixed use in the downtown core area and moderate residential density elsewhere in the community.
- (c) That employment areas be of sufficient size to provide a sound economic base for the community.
- (d) That development of open space nodes and community facilities be designed for active and passive recreation activities at the local level.
- (e) That significant natural features be preserved and enhanced.

#### ***Objectives - Land Use Relationships***

- (a) That the location and distribution of land uses be defined to enhance overall livability of the community.
- (b) That urban design be used to address the arrangement, appearance and functionality of mixed uses, particularly in the Aldergrove Core Area where high density residential and mixed-use are encouraged.
- (c) That regional traffic be separated from local traffic associated with the community.
- (d) That land use and circulation systems be integrated with existing and future development patterns.

## **7.2 ECONOMIC**

### ***Goal***

To provide the opportunity for individuals to realize their economic potential.

### ***Objectives***

- (a) The encouragement of orderly and efficient use of land having due regard for the efficient and economic provision of municipal services.
- (b) Development and progressive development shall be based on the servicing principle and an overall policy and master plan for servicing shall be prepared and adopted.
- (c) The establishment of a staging program for development that enables the provision of physical and social services to an acceptable standard.

## **7.3 ENVIRONMENTAL**

### ***Goal***

The establishment of a development pattern that incorporates the natural environment, preserves the existing aesthetic quality and provides for aesthetically attractive development of the community.

### ***Objectives - Environmental***

- (a) The provision of land uses in such a manner so as to control the pollution of air, land and water.
- (b) The development of land use patterns which will reduce and ensure the safety of residents from natural hazards.

### ***Objectives - Aesthetics***

- (a) The preservation of the beauty of the general topography and to control drainage.
- (b) The development, conservation and rehabilitation of structures and sites of good architectural quality.

### 1.0 LAND USE DESIGNATIONS

For the purpose of this Community Plan the Aldergrove Urban Area is divided into the following land use designations:

- Low Density Residential
- Medium Density Residential
- High Density Residential
- Commercial Areas
- Industrial Areas
- Recreation and Education Areas
- Conservation Areas

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The location of such land use designations are shown upon the plan document attached hereto and incorporated with and made part of this Community Plan.

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In addition to the above land use designations, the Aldergrove Core Area Plan in Part VI provides a list of land use designations specific to the areas within the Aldergrove Core Area defined in Schedule “A” of the Aldergrove Community Plan. These designations include:

- High Density Mixed Use
- Medium Density Mixed Use
- Service Commercial
- High Density Residential
- Medium Density Residential
- Low-Medium Density Residential
- Low Density Residential
- Parks, Recreation, and Greenspace
- Civic/Recreational

The location of the land use designations is shown in the Aldergrove Core Area Land Use Plan in Part VI.

### 2.0 AREA BOUNDARIES

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The plan boundaries for the Aldergrove Community Plan are defined in Schedule “A”.

The plan boundaries for the Aldergrove Core Area Plan are defined in Aldergrove Core Area Land Use Plan in Part VI.

### 3.0 INTERPRETATION

- 3.1 Deviations from the policies and plans of this document will require an amendment to the Community Plan.
- 3.2 It is intended that all figures, numbers and quantities within the Plan be considered approximate only.

- 3.3 Minor changes will be permitted to this Plan, policies and staging without amendments provided they do not affect the intent of the plan.
- 3.4 The Plan Map is intended to show general locations and distribution of major land uses and is designed to transmit the relationship between various land uses.

## 4.0 GENERAL PROVISIONS

The following apply to all land use categories.

### 4.1 ENVIRONMENTAL PROTECTION

- (a) No buildings, placement of fill or removal of soil shall be permitted within 7.0 meters (22.96 feet) of the top of ravines or natural water courses.
- (b) The natural flow regime and water quality of water courses shall be maintained in accordance with an approved storm drainage program.

### 4.2 DEVELOPMENT PERMITS

- (a) Development Permit Area "A" - Multi Family Residential

The areas shown as Development Permit Area "A" in Schedule "B" Aldergrove Development Permit Areas are designated development permit areas under Section 919.1 (1) (f) of the Local Government Act to establish objectives and provide guidelines for the form and character of development.

The objective of this development permit area is to encourage development of attractive multi-family areas appropriate to the overall character of Aldergrove. The development permit area guidelines for this area are:

- (i) Development shall be designed to integrate with and be compatible with adjacent development with respect to siting of buildings, exterior finish and design and landscaping.
- (ii) Building materials, massing and design shall support the creation of an attractive environment.
- (iii) The main pedestrian entrances to residential units shall face adjacent public streets to provide more friendly streetscapes and better surveillance and safety for the public streets.
- (iv) Rooflines of buildings shall be designed to screen rooftop equipment.
- (v) The use of perimeter berms, high fences and security gates shall be discouraged in order to provide for a more pedestrian friendly streetscape.
- (vi) Landscaping of the site shall be required to enhance the appearance of the development. Storage areas, garbage containers and other service areas shall be screened from adjacent roads and residential development. A landscaping plan shall be submitted as part of a development permit application. The landscaping plan shall show the location of the vegetation or trees to be retained, planted or removed and the type and size of materials to be used.
- (vii) Surface parking should be provided for in a number of smaller lots rather than one large lot. Provision of underground parking is encouraged. Attention shall be paid to landscaping within parking areas to soften the visual impact, especially where residential views extend over the parking area and where the parking area is visible from adjacent roads or residential development.

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- (viii) Internal roads shall have a coordinated streetscape of paving, lighting and tree planting.
- (ix) Signage shall be designed to be compatible with the design of the buildings and landscaping.
- (x) Watercourses shall be retained in a natural state and kept free of development, other than roads, walkways, trails or other public facilities. They shall be protected by restrictive covenant or dedicated for conservation use. The area to be protected or dedicated shall be defined by the Township in consultation with the Ministry of Environment and the Department of Fisheries and Oceans. Development shall be designed to provide leave strips adjacent to watercourses and shall be required to be set back from watercourses to protect the development and the ecological value of the area. A trail should be provided along Bertrand Creek.

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#### (b) Development Permit Area "B" - Commercial

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The areas shown as Development Permit Area "B" in Schedule "B" Aldergrove Development Permit Areas are designated development permit areas under Section 919.1 (1) (f) of the Local Government Act to establish objectives and provide guidelines for the form and character of development.

The objective of this development permit area designation is to ensure attractive commercial development along major roads and to reduce conflict with adjacent uses. The development permit guidelines for this area are:

- Buildings should be sited to consider adjacent development and to provide for pedestrian and traffic movement to adjacent properties.
- Buildings in areas adjacent to residential development should be low profile to reduce visual impacts on nearby homes.
- Buildings should be sited and designed so that sun penetration to roads and adjacent properties is maximized.
- Building elevations that are visible from adjacent roads, walkways or properties should be improved by painting, architectural details, landscaping or screening.
- Landscaping shall be required to enhance the appearance of the development and to screen parking, loading and outdoor storage areas from adjacent roads, residential and institutional uses and other commercial uses. A 5m wide landscaped area and a fence shall be provided adjacent to all areas designated for residential development in this plan. A landscaping plan shall be submitted as part of a development permit application.

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#### (c) Development Permit Area "C" - Industrial

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The areas shown as Development Permit Area "C" in Schedule "B" Aldergrove Development Permit Areas are designated development permit areas under Section 919.1 (1) (f) of the Local Government Act to establish objectives and provide guidelines for the form and character of development.

The objective of this development permit area is to encourage the development of an attractive industrial area. The development permit area guidelines for this area are:

- (i) New development should be designed to integrate with and be compatible with existing development with respect to siting, exterior finish and design of buildings and landscaping.

- (ii) New development should be street fronting with building elevations that provide visual interest. While parking areas may be located in front of the building, parking and loading shall mainly be in the rear.
- (iii) New buildings that are visible from adjacent roads or properties shall be finished and treated similarly to the front elevation.
- (iv) Rooflines of buildings shall be designed to screen rooftop equipment.
- (v) Lots shall be landscaped to enhance the appearance of the development and to screen parking, loading and storage areas and garbage containers from adjacent roads and residential development. A landscaping plan shall be submitted as part of a development permit application. The landscaping plan shall show the location of the vegetation or trees to be retained, planted or removed and the type and size of materials to be used.
- (vi) Signage shall be compatible with the building design and landscaping.
- (vii) Pursuant to Sec. 919.1 (4) of the *Local Government Act* a development permit under this section will not be required in connection with works related to a residential use on parcels zoned residential that are not otherwise being converted to an industrial use at time of application.

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- (d) Areas designated 'Comprehensive Development' shall be developed in accordance with the Development Permit Area "B" – Commercial guidelines for those buildings and structures abutting a road and Development Permit Area "C" Industrial Development permit area guidelines for those buildings and structures located on the interior of a lot.

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- (e) Development Permit Area "D" – Downtown Core Area

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The areas shown as Development Permit Area "D" in Schedule "B" Aldergrove Development Permit Areas are designated development permit areas under Section 919.1 (1) (f) of the Local Government Act to establish objectives and provide guidelines for the form and character of development.

The objective of this development permit area designation is to ensure attractive residential mixed use and commercial development in the downtown core. The development permit guidelines for the downtown core are listed in Appendix "A", Part VI of this Plan.

#### 4.3 SPECIAL CONDITIONS

- (a) Impact assessments may be requested by the Approving Authority where it is considered that the proposed development may be against the public interest as provided for under the Land Registry Act.
- (b) The impact assessment shall consider the potential impact of the proposed development on -
  - i) soil cover
  - ii) slope
  - iii) drainage
  - iv) economics and provision of servicing
  - v) need in relation to Municipal growth
  - vi) social and recreational services

of the proposed development on the immediate site, the Aldergrove Community and on Municipal Policies.

#### **4.4 REGULATORY BYLAWS**

Notwithstanding the provisions of this Community Plan specifying development criteria, the regulatory bylaws that implement this plan may contain provisions that are more restrictive.

## 1.0 RESIDENTIAL

### *Definition*

The residential land use designations on the plan map indicate areas in which residential dwellings will be the predominant use subject to the policies of the plan the following uses may be permitted:

Education, community public servicing uses, which by nature, scale, and design are compatible with residential uses, and open space and recreation uses.

### *Community Pattern*

- (a) Density of overall residential development shall not exceed five dwelling units per acre of gross residential area.
- (b) The minimum size parcel that may be created without all community services shall not be less than 1 hectare.
- (c) The following housing densities and types shall be permitted in those areas designated for residential use as low density, medium density and high density on the plan map as follows:

UNIT TYPE	NET DENSITY IN UNITS PER ACRE	GENERAL LOCATION POLICY
<b><u>Low Density</u></b> Conventional Single Family Conventional Cluster Duplex Cluster Semi Detached Zero Lot Line Mobile/Modular Home	Up to 9	Generally located at the interior of New Residential Neighbourhoods on Local or Residential Roads
<b><u>Medium Density</u></b> Four Plex Attached Single Family Patio or Court Housing Senior Citizen Housing	10 - 12	Generally located at the periphery of residential neighbourhoods adjacent to arterial roads
<b><u>High Density</u></b> Low Profile Apartments Walk Up Apartments Garden Apartments Senior Citizen Housing Medium Density Apartments High Density (Highrise) Apartments Medium Density Mixed Use High Density Mixed Use	13 – 18  Refer to the Aldergrove Core Area Plan in Part VI for permitted densities in the Aldergrove Core Area.	Generally located within and at the periphery of the Commercial Centre and should be physically well related to commercial land uses and existing stable residential areas. Located to or in close proximity to arterial roads.

- (d) The Corporation of the Township of Langley shall require the dedication of open space, in accordance with Sections 702 AA & 711 of the Municipal Act R.S.B.C. 1960, Chapter 255 and amendments thereto, as part of residential development.

In areas designated High Density Residential adjacent to the Commercial Centre, Council may consider densities up to 28 units per acre in the case of adult oriented residential developments and innovative infill housing proposals.

### ***Land Use Relationship***

- (a) Where residential uses are to be developed adjacent to designated industrial, commercial uses or major roads, consideration shall be given to requiring such building siting and design criteria and the provision of buffer devices as part of the residential development necessary to reduce audio, visual and pollution effects.
- (b) In the overall design and development of Residential Areas there shall be a gradual physical transition between housing types having significantly dissimilar physical design characteristics.

### ***Functional Design***

- (a) Elementary schools and neighbourhood facilities shall be located central to their service areas.
- (b) Direct access from residential uses to arterial roads shall be restricted.

### ***Economic***

The establishment of a staging program for development that enables the provision of physical and social services to an acceptable standard.

### ***Environmental***

The protection of the general topography and the control of drainage shall be encouraged in all residential land use development.

## **2.0 COMMERCIAL AREAS**

### ***Definition***

The commercial land use designation shall be a concentration and efficient integration of retail, service, cultural and recreation uses within a central core providing for the basic needs of the Aldergrove Urban Community.

### ***Community Pattern***

- The commercial use shall be the focal core of the Aldergrove Urban Community.
- The commercial use shall be located adjacent to arterial and/or collector roads.
- Priority shall be given to the development of the commercial use as a viable retail and service centre.

### ***Land Use Relationship***

- (a) Where the commercial use is adjacent to residential areas consideration shall be given to the following:
  - i) The buffering of buildings, traffic, parking or service areas.
  - ii) The orientation of traffic, parking and service areas flows away from the adjacent residential uses.

### ***Functional Design***

- (a) The number, location, spacing and design of vehicular access and egress points shall be regulated in order to avoid hazards to pedestrian and vehicular traffic.
- (b) Adequate, well designed off-street parking, loading and service areas shall be required in the commercial areas.

### ***Environmental***

The provision of adequate control of drainage shall be encouraged in all commercial land use development.

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## **2.5 TOWN CENTRE**

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The Aldergrove Core Area is defined in the Aldergrove Core Area Land Use Plan in Part VI. The community pattern, land use relationship, functional design, environmental considerations and other issues related to land use and urban design are addressed in the Aldergrove Core Area Land Use Plan.

## **3.0 INDUSTRIAL AREAS**

### ***Definition***

The industrial land use designation indicates areas which will be predominantly used for light and service industry.

### ***Community Pattern***

Priority shall be given to the development of the industrial land use area as a viable centre within the community and the Municipality.

### ***Land Use Relationship***

- (a) Consideration shall be given to:
  - i) the buffering of buildings, traffic, parking or service areas.
  - ii) the orientation of traffic, parking or service areas to major roadways where industrial uses are adjacent to rural, residential or commercial uses.
- (b) Consideration shall be given to the screening of outside storage areas from adjacent uses and roadways.
- (c) Consideration shall be given to the visual aesthetics emanating from structures, vehicular areas and outside storage areas.

### ***Functional Design***

- (a) The number, location, spacing and design of vehicular access and egress points shall be regulated in order so as not to hinder internal circulation or public roadway traffic.
- (b) Adequate off-street parking and off-street loading and service areas shall be provided as part of industrial development.

### ***Environmental***

- (a) Industrial uses that emit significant amounts of pollutants such as smoke, particle matter, odour or other toxic matter shall not be permitted.
- (b) The provision of adequate control of drainage shall be encouraged in all industrial development.

## **4.0 RECREATION AND EDUCATION AREAS**

### ***Definition***

The recreation and education land use designation indicates areas which provide lands for outdoor recreation, and education facilities in common or alone and shall provide for passive and active recreation and trail systems.

### ***Community Pattern***

- (a) The provision of community recreational and educational facilities which are easily accessible to the residents of the Aldergrove Urban Area.
- (b) The integration of educational and recreational facilities to provide for efficient usage of these facilities shall be pursued.
- (c) Open space and recreation areas shall be provided at the basic levels and standards as follows:

TYPE	SPACE REQUIREMENT RATIONS	MINIMUM SITE SIZE	RADIUS OF AREA SERVED
Local or Sub- Neighbourhood	0.5 acres/1000 Pop.	1 acre	0.5 miles
Neighbourhood	2.0 acres/1000 Pop.	5 acres	0.5 miles
Community	2.0 acres/1000 Pop.	10 acres	1.5 miles
Municipal	2.0 acres/1000 Pop.	-	-

### ***Land Use Relationship***

- (a) In the overall design of recreational and educational facilities consideration shall be given to locating these central to the service area.
- (b) Consideration shall be given to pedestrian walkways as part of recreational and educational facilities with the objective of providing safe and convenient access to residential, educational and recreational land uses.

### ***Land Acquisition***

Where land designated Recreation and Education areas are under private ownership, this plan does not imply that the recreation areas are open to the general public, or will be purchased by the Municipality or any other public agency.

## **5.0 CONSERVATION AREAS**

### ***Definition***

The conservation land use designation indicates areas which provide areas along natural water courses and ravines which require sound conservation practices.

### ***Community Pattern***

The provision of conservation areas adjacent to the natural boundary of all natural water courses.

### ***Land Acquisition***

Where lands designated Conservation Areas are in private ownership and application is made requesting a change in land use other than open space, due regard shall be given by Council to the following before amending the Official Community Plan:

- (a) that the lands do not form part of the protection area for natural water courses and ravines having due regard for topography, soil stability and vegetation cover.

- (b) Council shall give consideration to the costs and benefits accruing to the area by the public acquisition of the property; and
- (c) a comprehensive study shall be made of the property to determine the most desirable and practical function and use of this particular land.

### *Functional Design*

Watercourse protection shall be consistent with the Township of Langley Streamside Protection Bylaw.

### *Environmental*

The control of drainage shall be given due consideration in all open space and conservation area considerations.

## **Part IV – Servicing Policies**

### **1.0 PURPOSE**

The purpose of this section is to describe the servicing required to accommodate future growth within the Aldergrove Urban Area.

#### **1.1 WATER SUPPLY**

##### *General*

The Aldergrove Urban Area will be supplied by a Municipal Water Facility.

##### *Operational Policies*

- (a) The Aldergrove Urban Area shall be served with a municipal water system in accordance with municipal engineering standards.
- (b) A water system shall be installed in all residential, commercial and related development prior to development approval.

#### **1.2 SANITARY SEWER SERVICES**

##### *General*

The Aldergrove Urban Area will be served by a Municipal Sanitary Sewer Facility with effluent being directed to the Municipal Sewage Treatment Plant.

##### *Operational Policies*

- (a) The Aldergrove Urban Area shall be served with a Municipal Sewer System in accordance with municipal engineering standards.
- (b) Sanitary Sewers shall be installed in all residential, commercial and related developments prior to development approval.

#### **1.3 STORM SEWER SERVICES**

##### *General*

The Aldergrove Urban Area will be served by a Storm Water Facility with storm water eventually directed to Bertrand Creek.

##### *Operational Policies*

- (a) The Aldergrove Urban Area shall facilitate the dispersement of storm water in accordance with the drainage system and facilities identified by the Municipal Engineering Department.
- (b) Curbs, gutters and sewers as required shall be installed in all residential and commercial developments prior to final approval.
- (c) Improvements to the downstream facilities shall be incorporated to ensure minimal impact on the surrounding lands.

#### 1.4 TRANSPORTATION

The Aldergrove Urban Area shall provide a hierarchy of roads with the function, characteristics and associated criteria as follows:

DESIGNATION	RIGHT-OF-WAY WIDTH	FUNCTION
Arterial Streets	80 - 120 Feet	Primarily devoted to the movement of high speed; only rarely inter-related with adjacent land areas; vehicular access is restricted.
Collector Streets	60 - 80 Feet	These carry traffic from minor streets to the major system of arterial streets and highways, including the principal entrance streets of a residential development. They permit access to adjacent land areas, but generally do not permit long-distance through traffic.
Minor Streets	50 - 60 Feet	These carry traffic from collector streets to the individual land parcels within any given area. The primary function of these streets is to provide access to abutting properties.
Cul-De-Sac	500 Feet Max. 50 Feet Min. Radius	Dead end street with proper turning radius at end. Provides quiet residential street with no through traffic. Also helps solve difficult site problem with restricted access.
Lanes	20 Feet	These are minor ways that are used primarily for vehicular service access to the back or side of properties otherwise abutting a street. May be necessary in group, row houses or apartment developments. Not recommended in single family developments.

#### *Operational Policies*

- (a) Consideration shall be given to the provision of pedestrian walkways to facilitate access to schools, public facilities, convenience commercial uses, and transit stops.
- (b) Intersections of local streets with arterial streets shall be discouraged in the designs of new subdivisions.

- (c) Provisions shall be made in all site plans and plans of subdivisions for the proper relationship of buildings to the ultimate physical characteristics of the street.
- (d) The pattern and design of local and collector streets shall discourage medium and long distance trips from penetrating residential neighbourhoods.
- (e) The design and construction of all components of the transportation system shall be undertaken with due consideration for the disruption and despoliation of the natural environment that might occur.
- (f) Direct access to arterial roads of adjacent residential development shall be discouraged.
- (g) The inclusion of a landscaped buffer area between the arterial roadway and adjacent land uses shall be included in the development process.

## Part V – Implementation of the Plan

### 1.0 GENERAL

- (a) The purpose of the implementation part is to indicate the means available to achieve the objectives and policies contained in the plan. This plan will be implemented by both public and private development and all bylaws of The Corporation of the Township of Langley in conformity with the plan.
- (b) All studies, either by staff or commissioned shall be oriented to the policies and objectives of this Plan.

### 2.0 DEVELOPMENT STAGING

#### *Principle*

- (a) A basic objective of the staging program is to phase development contiguously, thereby bringing about an integral development pattern and a reduction of service costs.
- (b) The staging program shall remain flexible and allow for periodical adjustment to ensure that a variety of building locations, types and unit costs are offered to the future residents of the Aldergrove Urban Area and to prevent over extension of servicing in any one direction.
- (c) The staging program will provide a guide for various outside agencies such as the School District, B.C. Telephone, B.C. Hydro and the Ministry of Highways and Public Works in planning and budgeting their activities in the Aldergrove Urban Area.

### 3.0 CAPITAL PROGRAMMING

- (a) The capital expenditure program should be supported with project details through a capital construction program for an approximate period of not less than five years.
- (b) The implementation of the Community Plan is dependent on the establishment of capital construction and capital expenditure programs which translates the Municipality's policy into specific projects.
- (c) Project and financial conditions are not static and the capital expenditure program shall be revised annually.

### 4.0 MUNICIPAL REGULATIONS

#### *Zoning*

The use of land within the community plan area shall be implemented by a Zoning Bylaw drafted in accordance with Sections 702 - 707 of the Municipal Act R.S.B.C., Chapter 255 and amendments thereto.

## ***Subdivision***

The division of land within the Community Plan Area shall be implemented by a Subdivision and Development Control By-law drafted in accordance with Sections 711 - 713 of the Municipal Act R.S.B.C., Chapter 255 and amendments thereto.

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## **Part VI – Aldergrove Core Area Plan**

### **1.0 INTRODUCTION**

#### **1.1 Purpose**

The Aldergrove Core Area Plan (ACAP) describes the community's long term vision for the type, density and location of land uses desired for downtown Aldergrove to 2041. In addition to land use, the plan establishes objectives and policies for urban design, housing, mobility, servicing, heritage, the environment, and community amenities. Further, the plan defines downtown Aldergrove within the Township as a whole and in the context of its designation as a Municipal Town Centre.

The Aldergrove Core Area Concept Plan and Design Guidelines provide a comprehensive planning framework to accommodate an anticipated future downtown population of 5,000 residents through the creation of a highly sustainable and livable town centre.

#### **1.2 Planning Context**

Downtown Aldergrove is designated as a Municipal Town Centre in both Metro Vancouver Livable Regions Strategic Plan (LRSP) adopted in 1996, and the draft Regional Growth Strategy, currently undergoing consultation. Municipal Town Centres concentrate services oriented to the needs of a municipality and higher density housing, and encourage walking, cycling, and transit access to jobs, shopping and other destinations.

The Township of Langley Official Community Plan and the Aldergrove Community Plan reflect the LRSP designation in their policies for the downtown. The Aldergrove Core Area Plan provides more detailed information about the land uses, policies and issues that are relevant to the downtown, including a Land Use Map and associated policies which are implemented through zoning regulations and design guidelines.

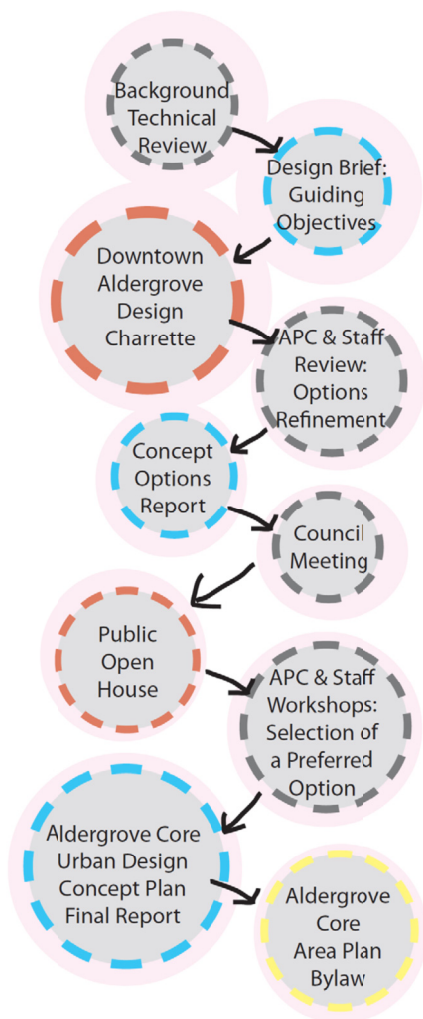
A number of other plans, initiatives and studies informed the planning process including:

- the 2004 Visioning Program which resulted in a council adopted vision and set of planning principles;
- the Township of Langley Sustainability Charter, adopted in 2008;
- an engineering study and commitment to upgrade the sewer by 2009 and the water supply by 2012 to service the projected downtown population to 2041; and
- the Master Transportation Plan, adopted in 2009.

### 1.3 Planning Process

This plan is founded on a strong base of community involvement that occurred throughout the core area planning process. The plan was created through a process that included a series of public events and design workshops. This included a preliminary workshop where planning goals to guide concept development were identified. This also included a design charrette where the community worked together with design professionals and Township staff to develop alternative concepts for the downtown. Subsequent workshops and a public open house lead to the identification of a preferred concept, which formed the basis for the development of the plan policies and design guidelines. The plan was developed over a period from May 2009 to May 2010.

Guiding and overseeing the process was the Aldergrove Planning Committee (APC) which is comprised of Aldergrove business and property owners, resident and stakeholder representatives and Council representatives. Regular APC meetings and workshops throughout plan development ensured a highly collaborative process.



**Aldergrove Core Area Planning Process Diagram**



**Township Sustainability Charter Diagram**



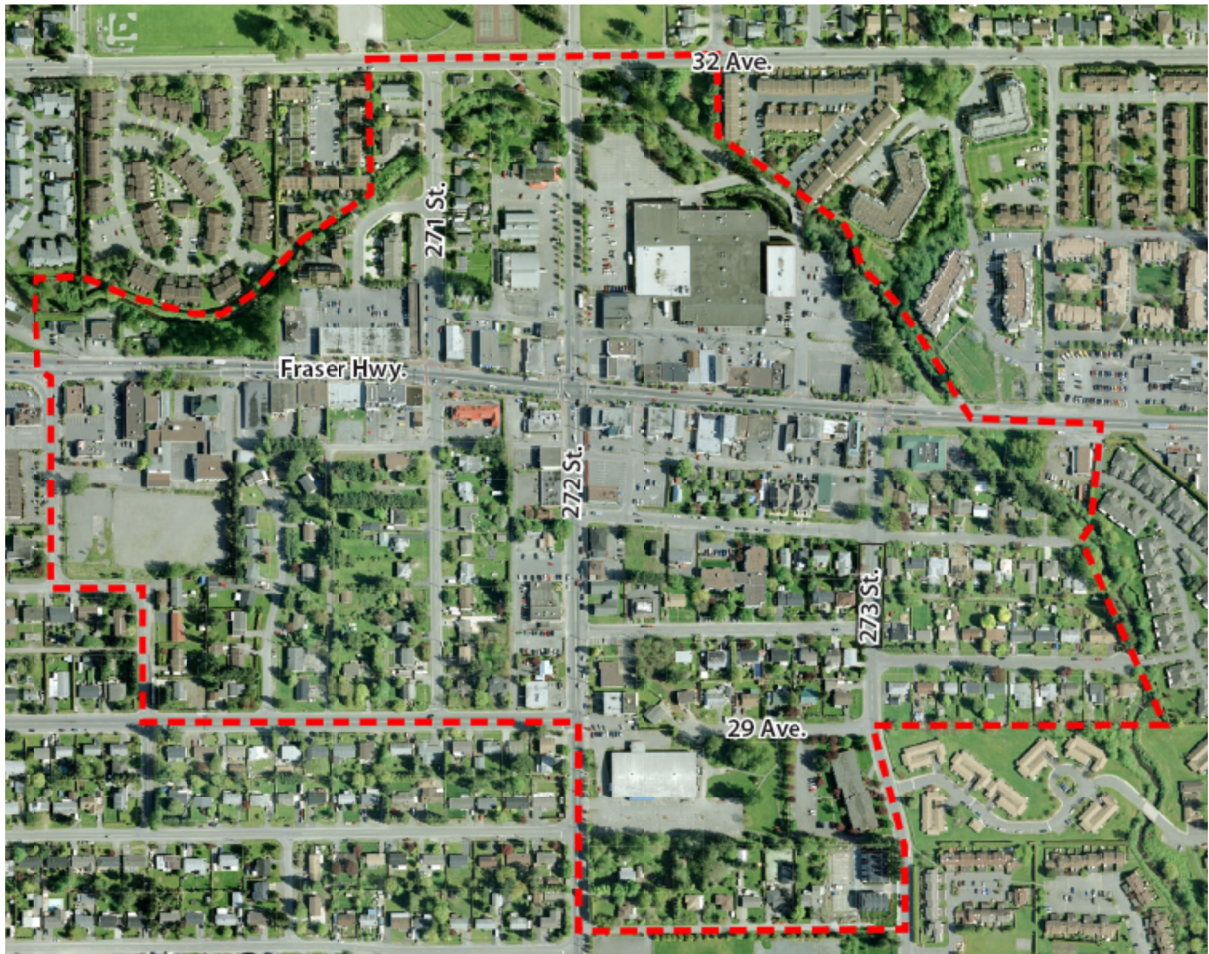
**The Design Brief**



**The Charrette**

## 1.4 Plan Area Overview

The Aldergrove Core Area is approximately 80 acres in size. It is centered on the intersection of Fraser Highway and 272 Street, and bounded by Bertrand Creek to the north and east, 29 Avenue to the south and 270 Street to the west. The Aldergrove Core Area is shown in the figure below.



Almost half of the plan area is bounded by the urban reach of Bertrand Creek. Bertrand Creek is ecologically significant and an important feature in the community. It is home to many fish species, including Coho salmon and endangered Salish Sucker and Nooksack Dace.

Aldergrove has its roots in farming and agriculture. Downtown Aldergrove is the traditional commercial and service centre for what used to be a predominantly farming community, but which has over the years transformed into a predominantly residential community supported by a range of primarily local serving commercial uses and with some industrial and agricultural/rural uses.

Today, the downtown continues to have a wide variety of commercial and service uses supporting a range of surrounding residential uses including single family homes, duplexes, townhouses and apartments. The majority of residential lands within the plan area are large lot single family homes ranging in age from 30 - 60 years old, with some townhouses and apartments including two fairly recent four-plexes located on 30th Avenue west of 272 Street.

Aldergrove's small town heritage is reflected in some of the built form and design elements of the downtown, including the location of buildings at the sidewalk edge, the heritage façade treatments of some buildings, and the pedestrian orientation and amenity of some of the buildings and public open spaces. Commercial uses are concentrated along Fraser Highway and 272 Street, and range in use from small local serving 'mum and pop' businesses, to larger commercial businesses and buildings, including the Aldergrove Mall. In 2009, there were approximately 120 active businesses and roughly 335,000 sq. ft. of commercial space within the study area. Due to the proliferation of new commercial/retail service centres outside of the downtown, the commercial vitality of the downtown is not what it once was, as evidenced by a number of vacant store fronts and the existence of some marginal businesses.

The large volumes of vehicular and commercial traffic at peak periods along the Fraser Highway impact the quality of fronting businesses and the adjacent pedestrian environment, despite the attractive and substantial streetscape enhancements that took place in the late '90s. The street system includes a limited network of lanes and is laid out in a modified grid pattern with an east - west orientation.



Shops along the Fraser Highway



New multi-family housing  
the downtown

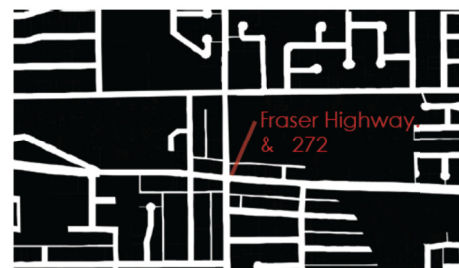


Figure Ground Diagram showing downtown  
block structure and street network

## 1.5 Planning Goals

These planning goals were developed by the community to guide implementation of the Aldergrove Core Area Plan. As such, they provide a broad overview of the planning framework for the downtown.



1. Reinforce downtown as the social, cultural and commercial heart of Aldergrove.
2. Increase housing choice and density in the downtown while ensuring a high degree of livability.
3. Incorporate mixed-use developments in the downtown including housing located above commercial uses.
4. Focus and limit the extent of commercial uses to create a vibrant commercial core.
5. Incorporate built form, layout and open space design that supports walking, transit-use and cycling for everyday travel needs.
6. Prioritize pedestrian activity in the core area to create a truly walkable downtown.
7. Create alternate route options for vehicles and goods movement traveling through the downtown core to minimize vehicle impacts on the Town's traditional commercial high street along the Fraser Highway.
8. Create an attractive and distinct public realm and interconnected open space network that emphasizes pedestrian comfort and safety and references the small town/rural identity of the community.
9. Create a greener downtown that provides parks, street trees and protects and enhances Bertrand Creek as an ecological feature and focal point.
10. Incorporate built forms, open space treatments and innovative infrastructure that reduces overall environmental impacts.
11. Strive for innovation, vision and boldness in the downtown as it redevelops to achieve true community wide sustainability and livability to 2041 and beyond.

## 1.6 Plan Overview

The Aldergrove Core Area Plan, along with the Concept Plan and Design Guidelines, represent a vision to transform downtown Aldergrove into a compact, mixed-use, highly walkable and transit oriented town centre. The land use framework as illustrated by both the land use plan and the concept plan creates significant additional residential capacity and a good mix of housing types through the introduction of medium and higher-density residential development in both ground-oriented housing and apartments. The plan is further premised on maintaining and enhancing the Fraser Highway and 272 Street as vibrant commercial streets, with a mix of shops, services, cafes and restaurants to serve and provide jobs for local and regional residents. While the plan encourages the introduction of a number of tall buildings, a human scale is emphasized through a built form and character that emphasizes pedestrian uses, comfort and safety.

The plan is premised on the creation of great streets and public open spaces through a high quality of design and a positive interface between the public and private realm at the street level. Pedestrian oriented and humanly scaled streets and plazas, and a highly connected pedestrian network, are other essential elements of the plan which, together, are intended to foster a functional, aesthetic and vibrant downtown Aldergrove. A reference to downtown Aldergrove's past is achieved through a modern interpretation of historic form and character elements as illustrated in the design guidelines. These are intended to foster creative and innovative design responses to sustainability, livability, and sense of place, and through public art referencing Aldergrove's agricultural roots.



*Aldergrove Core Area Concept Plan (for illustrative purposes only)*

Other key elements of the plan include:



**Downtown Greenway**



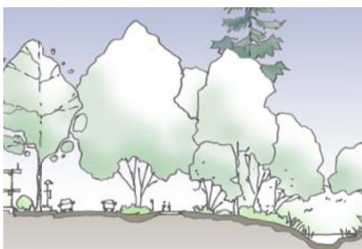
**Pedestrian-oriented Streets**



**Active Retail Streets**



**Downtown Aldergrove  
Transit Plaza**



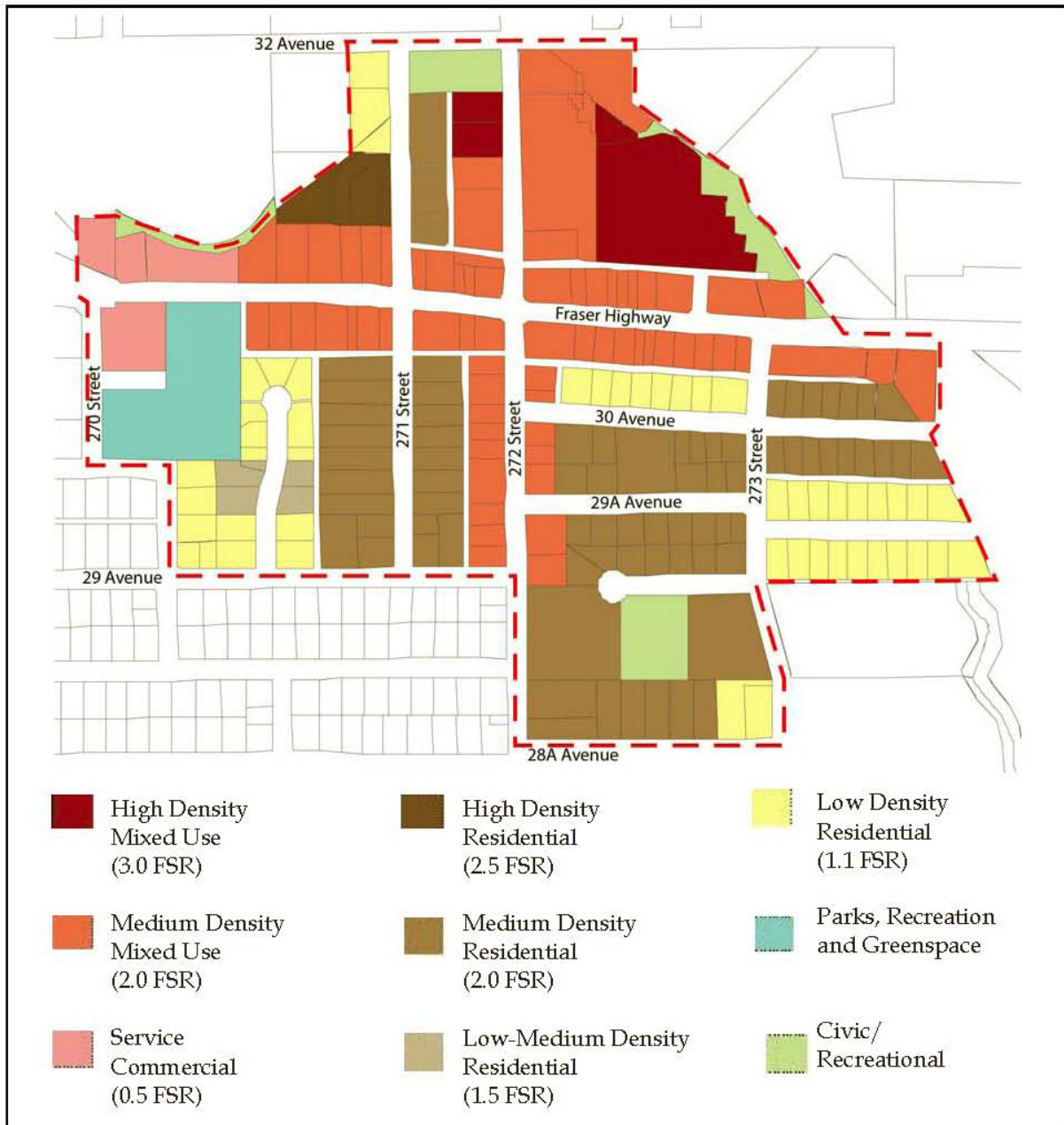
**Bertrand Mews**

- **on and off-street greenways** that emphasize comfortable, safe and convenient pedestrian travel and activities within the downtown, and connect to a wealth of parks and other amenities located immediately adjacent to the downtown;
- **new street network connections** to create more route options for pedestrians;
- **Bertrand Mews**, a new small scale, one-way local street running parallel to Bertrand Creek with primarily residential frontage and extensive traffic calming measures;
- **mid-block paseos** or pass throughs to create pedestrian connections to main commercial streets and through the downtown;
- **a new civic/recreational precinct** located on the Aldergrove Elementary School site that includes a pool and ice rink;
- **a transit plaza** located adjacent to the main bus exchange, lined with shops, cafes and other active uses, containing pedestrian amenities, public art, and able to accommodate outdoor community events including a regular farmers market;
- **pedestrian crossings** at key locations throughout the downtown; and
- **urban parks and plazas** located throughout the downtown.

## 2.0 ALDERGROVE CORE AREA LAND USE PLAN

The land use plan and associated land use designations below and on the following page describe in detail the community's long term vision for the type and intensity of land uses desired for the downtown. Together, these will guide the re-zoning and development process. Density is defined using **Floor Area Ratio (FSR)**, the ratio of the total floor area of buildings on a certain location to the land area (space) of that location.

### 2.1 Land Use Plan



Aldergrove Core Area Land Use Plan

## 2.2 Land Use Designations

Designation	Density	Height	Built Form Examples
 High Density Mixed Use	3.0 FSR	up to 16 storeys (50 m)	  
 Medium Density Mixed Use	2.0 FSR	up to 5 storeys (14 m)	  
 High Density Residential	2.5 FSR	up to 16 storeys (50 m)	  
 Medium Density Residential	2.0 FSR	up to 6 storeys (20 m)	  
 Low-Medium Density Residential	1.5 FSR	up to 4 storeys (14 m)	  
 Low Density Residential	1.1 FSR	2.5 storeys	  

### 3.0 PLAN POLICIES

The objectives and policies in this section outline the strategies for achieving the goals identified in Section 1.5. and for implementing the Land Use Plan in section 2.0.

#### 3.1 Land Use and Development

**Objective [1]:** To create a compact, mixed use and walkable town centre

**Objective [2]:** To accommodate an anticipated total future population of approximately 5,000 people by 2041.

##### Policies

- 3.1.1 Facilitate a mix of land uses including residential, office and retail commercial, mixed use, and civic institutional.
- 3.1.2 Accommodate a significant share of anticipated future residential growth in Aldergrove through multi- family and mixed use developments in the downtown plan area. This aims to support continued revitalization of downtown Aldergrove to:
  - reinforce its status and function as a pedestrian and transit oriented Municipal Town Centre as designated by Metro Vancouver Draft Regional Growth Strategy;
  - enhance the viability of existing and future businesses in the downtown; and
  - reduce development pressure on natural open spaces, agricultural and rural lands in other parts of Aldergrove and the Township as a whole.
- 3.1.3 Encourage a range of businesses and services including retail, offices, and entertainment uses in the downtown to balance housing and jobs, provide economic opportunities, and enhance the vibrancy and safety of the downtown.
- 3.1.4 Introduce supportive zoning schedules where the existing zoning bylaw does not sufficiently cover the intent, permitted uses, densities, and technical and design considerations of this plan.

**Objective [3]:** To reinforce the downtown as the cultural, social, and commercial heart of Aldergrove by focusing specialty retail and complementary entertainment, restaurant and civic uses along Fraser Highway and 272 Street.

##### Policies

- 3.1.5 Encourage location of specialty retail, entertainment, restaurants and services along 272 Street and Fraser Highway.
- 3.1.6 Encourage a mix of entertainment and other active uses in the downtown including shops, restaurants, cafes and bars to create activity during and after usual business hours.
- 3.1.7 Increase service and office uses along commercial streets, preferably above ground floor retail, to help create a balance of jobs and housing.
- 3.1.8 Encourage, where appropriate, larger retail stores up to 3,300 sq. m. to anchor the downtown area and support the integration of other specialized businesses. At least one large (3,000 sq. m. +) grocer is desired as a key amenity within walking distance for downtown residents.
- 3.1.9 Retail shops are encouraged to integrate into mixed use buildings with residential uses located above to contribute to a more compact, mixed use and pedestrian oriented downtown.
- 3.1.10 Promote recycling and other best practices in waste management among downtown businesses and support programs that reduce the need for dumpsters to contribute to a safe, clean environment in the downtown.

**Objective [4]:** To encourage new developments to use best management practices for water and energy conservation.

**Policies**

- 3.1.11 Minimize energy requirements of new buildings through design and building orientation and through the adoption of energy efficient fixtures.
- 3.1.12 Promote district heating and renewable energy use (e.g. solar and geothermal) and explore opportunities to allow new developments to be retrofitted in the future.
- 3.1.13 Strengthen water conservation programs, which include water use reduction fixtures in buildings and landscape designs that favour drought-resistant native plantings and a minimal lawn area.

**Objective [5]:** To improve watershed ecological health by protecting and enhancing Bertrand Creek.

**Policies**

- 3.1.14 Undertake a habitat protection and enhancement plan to guide improvements to and development of the Bertrand Creek corridor.
- 3.1.15 Define streamside protection and enhancement areas for Bertrand Creek, in accordance with the Streamside Protection Bylaw No. 4485, provincial regulation and federal policy.
- 3.1.16 Encourage enhancement of in-stream and riparian habitat.

**3.2 Access and Mobility**

**Objective [1]:** To prioritize walking, cycling, and transit use within the downtown and to create safe, convenient, and pleasant access for people of all ages and abilities while accommodating vehicle travel and access to businesses, residences, and other downtown destinations. A compact urban form, a mix of land uses and alternative mobility options will be promoted to reduce the need for cars in downtown Aldergrove.

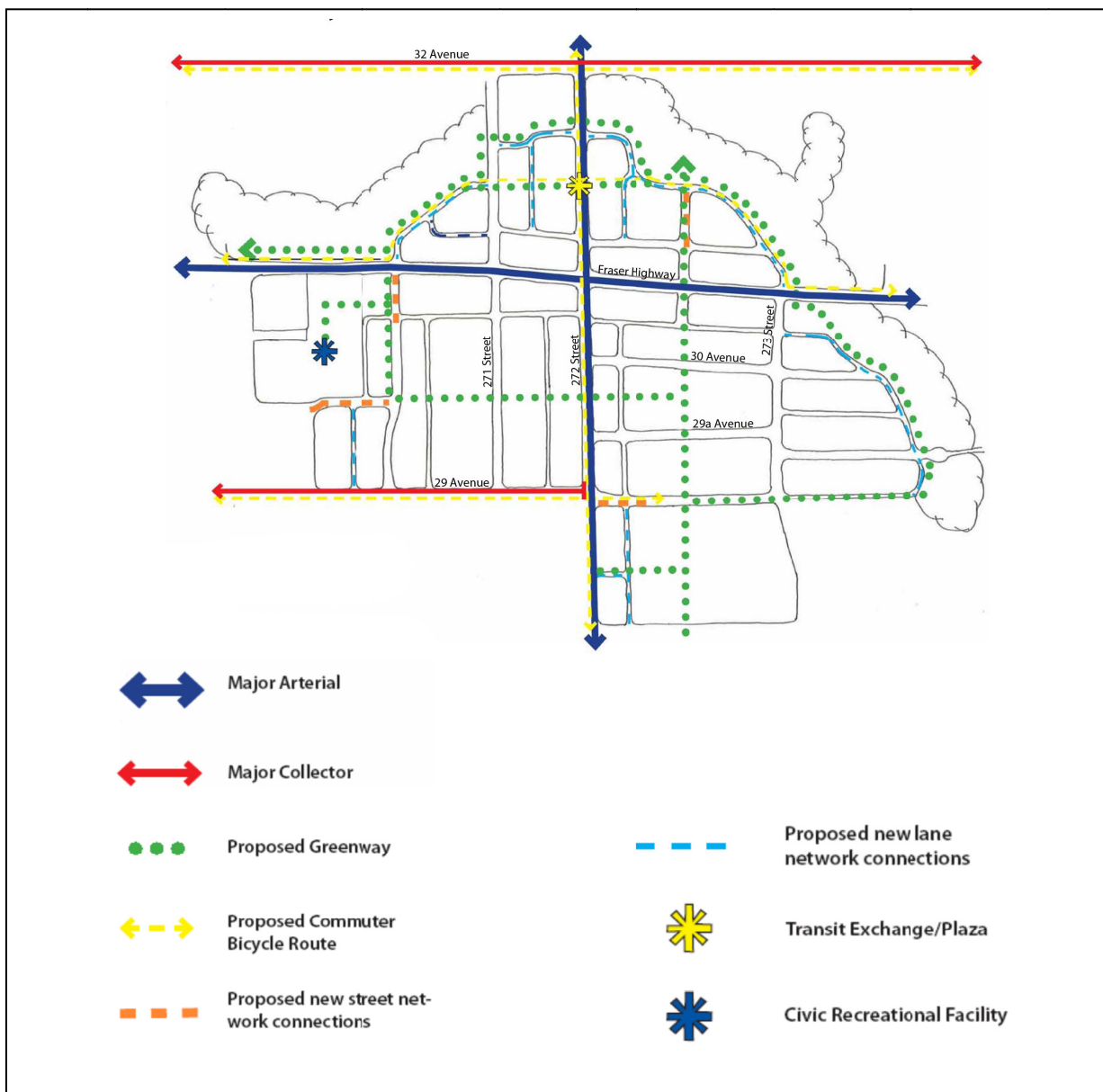
**Non-vehicular**

**Policies**

- 3.2.1 Shared use between pedestrian and cyclists will be emphasized in all future public and private realm development to create a healthier and more livable downtown.
- 3.2.2 Priority will be given to shared use travel, acknowledging that transit users are also pedestrians. All future private and public developments will reflect this priority.
- 3.2.3 New connections to the downtown street network will be achieved by securing rights-of-way and new network connections as opportunities arise through the redevelopment process.
- 3.2.4 Increased shared use connectivity is a priority. New publicly accessible off-street shared use greenways and trails will be secured as opportunities arise through the redevelopment process. Priorities for new pedestrian greenways and trails are indicated in the Mobility

Framework Diagram.

- 3.2.5 Equip designated bicycle routes as indicated in the Mobility Framework Diagram with either shared or dedicated on-street lanes along with:
- adequate signage,
  - marked lanes for both shared and dedicated lanes, and
  - cyclist activated signals at key intersections.
- 3.2.6 Increase pedestrian interest, comfort and safety by incorporating public art, street trees, outdoor furniture, ornamental plantings, pedestrian lighting, human scale architecture, and by orienting buildings towards the public realm through the use of transparent shop front windows, balconies, and other features that increase surveillance and “eyes on the street”.



*Mobility Framework Diagram*

## **Transit**

- 3.2.7 Downtown Aldergrove is envisioned to have a significant transit orientation achieved through transit supportive densities and urban design that foster safe, direct and convenient connections to transit stops.
- 3.2.8 An enhanced transit exchange, with adjacent plaza, bus parking bays (with limited layover function) and a range of pedestrian amenities, is envisioned along 272 Street north of the Fraser Highway, as identified in the Concept Plan and the Mobility Framework Diagram. Development fronting onto the plaza will incorporate active, publicly oriented uses at ground level to ensure a pedestrian orientation.
- 3.2.9 The Township will continue to work with Translink to ensure a frequent, convenient and effective transit service to and from downtown Aldergrove as its population of residents and workers grows as envisioned by this plan.

## **Fraser Highway**

**Objective [2]:** To reinforce the function and character of Fraser Highway as Aldergrove's traditional main street while accommodating vehicular traffic and goods movement to, from, through and within the downtown.

## **Policies**

- 3.2.10 Fraser Highway will have an enhanced pedestrian orientation while continuing to accommodate traffic and goods movement traveling to, from, through and within the downtown.
- 3.2.11 Vehicular capacity along Fraser Highway will be increased at peak periods in peak directions by removing on-street parking at peak periods in peak directions of traffic flows.
- 3.2.12 Full signalized intersections will be provided at the following three intersections:
  - Fraser Highway and 271 Street,
  - Fraser Highway and 272 Street, and
  - Fraser Highway and 273 Street.
- 3.2.13 Pedestrian signalized intersections will be provided at the following intersections:
  - Fraser Highway and 271 Street, and
  - Fraser Highway and 272A Street.
- 3.2.14 The intersection at 272 Street and Fraser Highway will be enhanced to include left turn lanes in all directions, and two travel lanes in each direction (west and east) along Fraser Highway. A widening of the public right-of-way will be required to accommodate these improvements.
- 3.2.15 A detailed streetscape design and engineering study will be undertaken for Fraser Highway to implement the mobility network and infrastructure improvements identified in policies 3.2.10 to 3.2.14, inclusive.
- 3.2.16 As part of the overall update to the Aldergrove Community Plan, alternate routes to Fraser Highway will be identified to reduce the impacts of regional traffic on the downtown, consistent with the Recommended Future Road Network in the Master Transportation Plan.

## **Parking and Transportation Demand Management**

**Objective [3]:** To ensure parking in the downtown balances the need for auto storage and the goals of compact development, alternative transportation and an attractive downtown.

**Objective [4]:** To manage mobility demand by encouraging private and public developments to support non-automobile travel.

### **Policies**

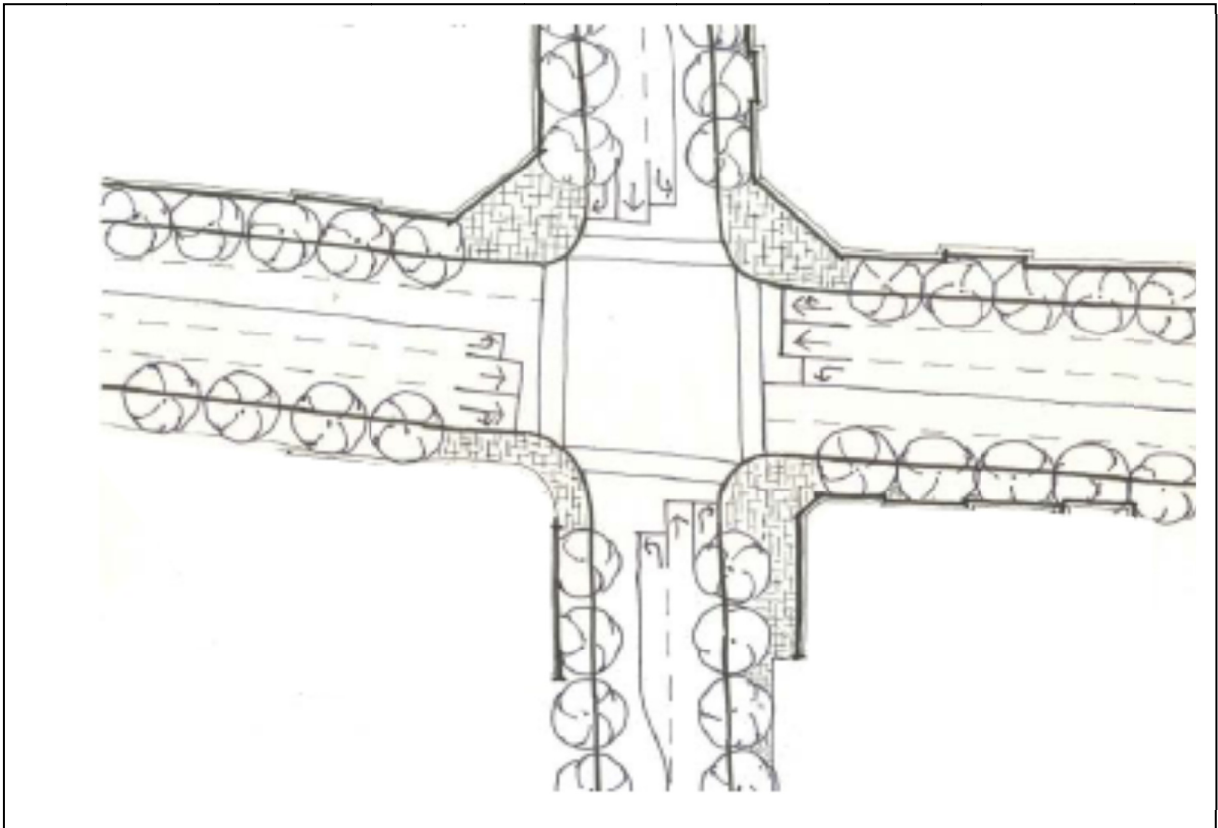
- 3.2.17 Support alternative means of mobility with facilities such as bicycle racks.
- 3.2.18 Consider a reduction in parking requirements for residential developments in the downtown, where:
  - Transportation Demand Management (TDM) measures such as transit passes for residents and employees, bicycle storage and facilities, and shared car programs for new development proposals are established; and/or
  - Connections to pedestrian and bicycle networks and public transit services are provided.
- 3.2.19 Encourage shared parking between projects where it can be justified and where it does not generate negative impacts. A traffic study by a qualified engineer is required where shared parking is proposed.
- 3.2.20 Provide on-street parking wherever possible along both sides of commercial and residential developments, including spaces for physically challenged people at strategic locations.
- 3.2.21 Street fronting (off-street) surface parking is not permitted along primary and secondary retail streets, and is strongly discouraged throughout the entire downtown plan area.
- 3.2.22 Encourage shared parking facilities where parking demand varies over the course of day for different activities and land uses.
- 3.2.23 Promote alternatives to surface parking, such as a central, shared structure for non-residential parking, or car co-ops.
- 3.2.24 Consider central parking structures, where proven feasible and appropriate, at sites:
  - that can accommodate a ground-floor commercial frontage if located on a commercial street,
  - that are strategically located,
  - that are near significant trip generators (e.g. the proposed civic/recreational facility)
  - that are compatible with adjacent existing or planned residential uses (to avoid detrimental lighting and traffic impacts),
  - where resulting traffic patterns will not negatively impact pedestrian and cycling networks,
  - that can be redeveloped in the future if parking needs decline, and
  - where “green” elements can be accommodated.
- 3.2.25 Require extensive landscaping and natural storm water infiltration on all off-street surface parking facilities.
- 3.2.26 Screen or otherwise render unobtrusive off-street parking from public view.

- 3.2.27 Require a traffic impact study to accompany each major development application. The study will recommend appropriate measures for turning, pedestrian crosswalks, signalization, access, servicing, and parking. Development applications that include tall buildings (6+ storeys) and/or large sites (5,000 m<sup>2</sup>+) are considered major.
- 3.2.28 A parking strategy for the downtown should be undertaken to determine off-street and on-street parking requirements and standards in the context of achieving the broader local economic development, livability and sustainability goals, objectives and policies set out in this plan.

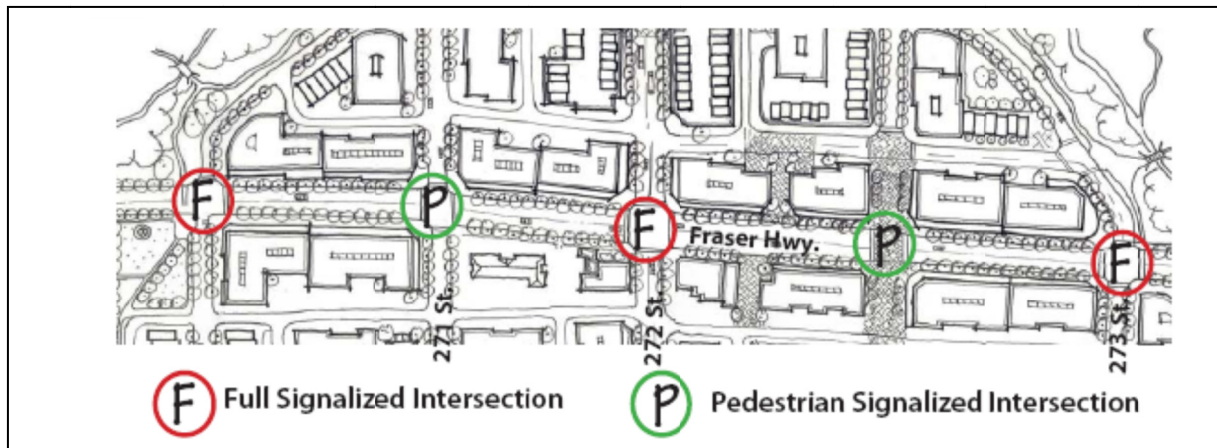
**Objective [5]:** To minimize the impact of the mobility network on the environment while following an economical approach to infrastructure improvements.

### **Policies**

- 3.2.29 Maintain existing rear lanes, and create new ones where opportunities exist, with green features, including natural drainage.
- 3.2.30 Introduce a street tree program that includes stormwater and pollution management as objectives in addition to beautifying and shading.
- 3.2.31 Incorporate rain gardens and swales (linear rain gardens) in roadside landscaping, where practical, to slow, store, and filter stormwater before discharging it to the drainage system.



*Proposed Intersection Design Concept for Fraser Highway at 270 Street, 272 Street and 273 Street.*



*Proposed Fraser Highway Intersections by Type – for illustrative purposes only.*

### 3.3 Housing

**Objective [1]:** To provide diverse and appropriate housing types to meet the needs of a growing downtown population for a range of income levels, lifestyles, ages and abilities

**Objective [2]:** To increase affordable, rental and special needs housing in the downtown.

#### Policies

- 3.3.1 Encourage a mix of ground-oriented units and apartments and a variety of unit sizes for different household types and income groups within individual multi-family developments and throughout the downtown.
- 3.3.2 Prioritize development proposals that address known housing gaps in Aldergrove and in the Township as a whole. In particular, consideration will be given to multi-family proposals for families with young children (i.e., ground-oriented housing), singles and worker housing (i.e., compact, rental housing), supportive housing and multi-level care facilities that allow people to remain in the community as they age.
- 3.3.3 Social and environmental goals identified for the downtown, particularly with regards to the provision of seniors housing, affordable rental housing, low impact and energy-efficient site and building design, and/or new greenways and open space, will be encouraged through the redevelopment and rezoning process.
- 3.3.4 Encourage a social mix within all neighbourhoods throughout the community through mixed housing developments (by type, tenure and design) while conforming to the residential designations of this plan.
- 3.3.5 Support alternative housing tenures such as co-housing and cooperative housing, especially where they meet an affordable housing need.
- 3.3.6 Work with community groups and other agencies to increase well-managed, supportive or transitional housing for special needs groups, especially the physically or developmentally disabled, young people and others with unique social and health needs.
- 3.3.7 Develop partnerships to create innovative housing that is accessible to residents with low incomes and/or special needs.

**Objective [3]:** To encourage high quality design in all residential projects that creates a positive interface with the public realm, remains compatible with the form and character of the existing homes, and maximizes the livability of individual units.

#### **Policies**

- 3.3.8 Promote housing designs that present a “friendly face” to the street or walkway. This can be achieved with well defined entrances that are welcoming, clearly visible and accessible from the street, and through street edges that are characterized by low, neighbourly fences and landscaping.
- 3.3.9 Maximize opportunities in residential projects for creating usable and attractive private and semi-private outdoor amenity spaces, and in particular, play areas for children that have surveillance and direct access from ground oriented as well as upper storey units. This can be achieved by incorporating open spaces, court yards, patios, balconies, upper storey terraces and roof top gardens.
- 3.3.10 Require residential development proposals to submit:
  - view and shadowing impact studies showing how the impact of developments over four storeys in height may be minimized on adjacent properties; and
  - public realm plan that coordinates private improvements to the streets and connects semi-public adjoining spaces with the public right-of-way.
- 3.3.11 Encourage residential development proposals to include a green roofscape plan.

### **3.4 Public Open Space, Heritage, Community and Culture**

**Objective [1]:** To ensure adequate park facilities for downtown residents.

**Objective [2]:** To increase green space and create a network of trails and greenways to enhance livability and character.

#### **Policies**

- 3.4.1 Prioritize the acquisition and development of parks in areas identified in the Aldergrove Core Area Concept Plan and in areas of medium to high density residential growth as identified in the Aldergrove Core Area Land Use Plan;
- 3.4.2 Procure parkland by purchase, grants, conservation easements, development cost charges (DCCs), and the subdivision approvals process, where feasible, for the purpose of providing additional park and recreation lands and facilities, and open space, as indicated by the objectives and policies of this plan, and by other ongoing parks planning initiatives.
- 3.4.3 Consider land trade or sale of Township owned properties for more suitably located sites for the development of parks, civic and recreational facilities.
- 3.4.4 Create direct shared use greenway connections to parks, schools, and other cultural and recreational facilities adjacent to the downtown plan area.
- 3.4.5 Design greenways and trails as physical, visual and habitat connections to other parks, natural areas and regional trail networks, including recreational staging areas outside the plan area.
- 3.4.6 Develop options and guidelines for alternative stormwater management practices that are compatible with greenway development.

**Objective [3]:** To enlist the support of new development in increasing publicly accessible open space and green space in the downtown.

### **Policies**

- 3.4.7 Consider the inclusion of portions of private property for publicly accessible greenway development, particularly where it creates a safe and pedestrian friendly mid-block connection or alternate corridor, or is an area of recreational or environmental value.
- 3.4.8 Secure easements for pedestrian access in areas where deemed necessary.
- 3.4.9 Require mixed-use commercial developments to give special attention to the articulation and creation of common green spaces, plazas, courtyards, seating/waiting areas or widened sidewalk areas that would create vibrant street life (cafes, food/flower stands, etc.).
- 3.4.10 Encourage mixed use commercial projects to dedicate residential green space. Options may include green roofs, courtyards, communal gardening areas, greenways, and other common amenity areas.
- 3.4.11 Promote pedestrian friendly designs among developments located adjacent to all public open spaces and greenways in the downtown.

**Objective [4]:** To conserve identified heritage buildings and landscapes of interest through the preservation, restoration, renovation, and/or adaptive re-use of these resources, and consider their form and character in the development of surrounding sites.

### **Existing Heritage Resources in Downtown Core**

Within the Aldergrove core, there are landmark buildings that both reflect, and have come to symbolize, the community's early roots from the early 1900s. The following sites are listed on the Township of Langley Heritage Inventory:

- F.J. Hart Building/BC Telephone Exchange (c. 1910), located at 3190 271 Street, an historic site designated by bylaw in 1992;
- Aldergrove Presbyterian (later United) Church (b. 1911), located at 2940 272 Street, historic site; and
- Aldergrove Elementary School (b. 1913), located at 27032 Fraser Highway, heritage character site.

### **Policies**

- 3.4.12 Retain identified listed sites in recognition of their historic value to the community and for the cultural enjoyment for present and future generations.
- 3.4.13 Encourage owners to maintain the historic style and character of these buildings in undertaking future changes.
- 3.4.14 Ensure that the scale of these buildings and the surrounding environment is considered when preparing zoning bylaws and reviewing development applications for adjacent proposals in the area.
- 3.4.15 Utilize heritage conservation instruments, such as heritage revitalization agreements, to vary density, siting and other regulations, in order to achieve desirable development outcomes that will integrate old forms with new development on a site.

- 3.4.16 Consider heritage incentives in the form of residual density or transfer of density for the developments of sites that incorporate heritage preservation strategies.
- 3.4.17 Require that the future revitalization of these sites be undertaken in accordance with the current best conservation practices, such as the accepted Standards and Guidelines for the Conservation of Historic Places in Canada.
- 3.4.18 Develop publicly accessible interpretive signage that recognizes the historical significance of the Aldergrove Elementary School as part of the site's redevelopment in future.
- 3.4.19 Establish a list of other significant landscape features not already listed to be considered for retention in the area in future.
- 3.4.20 Explore the need for an interpretive signage program that focuses on the historic highlights of Aldergrove.

**Objective [5]:** To reinforce gateways and distinctive points in the downtown as landmarks for way finding and reinforcing its unique identity and sense of place.

#### **Policies**

- 3.4.21 Highlight focal points and gateways with symbols of entry, public art, signage, landscaping, building or view corridor preservation, distinctive architecture, road pattern, street furniture, activated pedestrian signals, and traffic calming, where appropriate.
- 3.4.22 Create a variety of building forms by varying the height of roof pitches and use of materials that interpret built and human history of the community.
- 3.4.23 Support and work closely with the business community in developing a streetscape theme for its coordinated public art program with banners, adequate lighting, special sidewalk treatments, and bench and trash receptacles, and street tree and landscaping plans.
- 3.4.24 Recommend that new developments incorporate clear directional signage.

**Objective [6]:** To provide adequate community facilities, programs and social amenities for the growing downtown population.

#### **Policies**

- 3.4.25 Encourage civic, recreational, community and institutional uses in the areas designated for Residential, Commercial, and Mixed Use.
- 3.4.26 Consider the Aldergrove Elementary School site and building for civic, recreational and community services that may include a community centre, park, pool and arena, and appropriate protection of the original Aldergrove School.

**Objective [7]:** To ensure downtown places, activities, uses and services are socially inclusive through the support of diverse community needs, especially in arts and culture while reinforcing the downtown as the heart of Aldergrove.

#### **Policies**

- 3.4.27 Encourage families to live downtown by providing a range of family housing, child friendly green and recreational spaces, schools, day cares, and family oriented institutions and services.

- 3.4.28 Create an accessible and welcoming public realm in the downtown through physical design and an emphasis on strategic locations such as gateways and points of community interest.
- 3.4.29 Advocate for a diverse and multicultural downtown and support inclusive and culturally sensitive community engagement processes, gathering places, programs and services.
- 3.4.30 Foster the growth of arts and culture in the downtown, including such activities as public markets, public concerts, local community celebrations and installation of public art that reflects Aldergrove's history and sense of place.

**Objective [8]:** To enhance safety in the public realm by promoting Crime Prevention through Environmental Design (CPTED) principles and adequate property maintenance standards.

#### **Policies**

- 3.4.31 Enforce current property maintenance standards and strengthen them where permitted by the provincial legislation.
- 3.4.32 Continue with the Township's current program of reducing graffiti and vandalism to keep the downtown clean and dignified.
- 3.4.33 Reduce places of concealment through improved landscaping and sightlines.
- 3.4.34 Require new developments to provide adequate lighting along their street fronts and at entrance ways and all other pedestrian areas.
- 3.4.35 Promote designs that facilitate natural surveillance by retaining clear sightlines between public pedestrian areas and the building entrance for those who park there and for occupants of nearby buildings.
- 3.4.36 Recommend new developments to design and maintain landscaping that allows visibility (such as short shrubs and pruned trees).
- 3.4.37 Encourage new development to use ornamental grill creatively in fencing or securing ground floor windows.
- 3.4.38 Enhance casual surveillance and "eyes on the street" through strategic placement of windows, balconies and street level uses in new developments.
- 3.4.39 Strengthen natural surveillance of parks, open spaces, and children's play areas by ensuring new developments adjacent to them are designed accordingly.

**Objective [9]:** To ensure the public realm is accessible to all residents, especially the socially disadvantaged, the handicapped and the elderly.

#### **Policies**

- 3.4.40 Review all public realm elements for accessibility such as access to buildings, parking areas, and parks, and design public amenities for all users, eliminating the need for adaptation, customization, or separation of groups with different abilities.
- 3.4.41 Adopt "universal design" approaches to accommodate diverse individual preferences thus avoiding stigmatization of any user groups. Provisions for privacy, security, and safety should be equally available to all users.
- 3.4.42 Design all pedestrian routes to be safe and easy to use by persons with disabilities. Generally, such routes should be easily identifiable, clearly separated from vehicular routes, and free of obstacles at all times of the year.

- 3.4.43 Make all areas and special features of a site or facility, normally used or available to members of the general public, accessible to and usable by people with disabilities.
- 3.4.44 Design public parks and playgrounds for use by people with varying abilities.
- 3.4.45 Adapt pedestrian paths and footbridges to be used by persons using various mobility aids.
- 3.4.46 Adopt universally accessible designs for all amenities adjunct to paths and walkways, and position them to one side of the walkway surface.

## **4.0 IMPLEMENTATION**

Implementation of the Aldergrove Core Area Plan requires the collaboration of the Township of Langley, property owners, developers, as well as residents and business owners. The key implementation tools for this plan (and for guiding the re-zoning and development permit process in the core area) are:

- the Land Use Plan and associated Land Use Designations, along with the objectives and policies recommended in this plan, adopted as a section in the Aldergrove Community Plan;
- the Concept Plan and Design Guidelines; and
- the Township of Langley Zoning Bylaw No. 2500.

### **4.1 Incentives**

The Aldergrove Core Area Land Use Plan has garnered momentum and support for an area concept that will see an expanded mix of land uses, density and economic diversity in the area. The Township of Langley is committed to fostering a successful implementation and transition process.

The next steps to revitalization will gradually include utility upgrades, public realm investments, development approvals and private investment in the area. A combination of tools can be used to facilitate this process.

The choice of incentives and revitalization tools fosters certainty and clarity for development and investment in the community. Investment is encouraged when clarity is built into the community's planning and development policies and guidelines, subdivision and development approval processes, as well as in financing, cost recovery and infrastructure implementation strategies. Stability builds confidence in the development industry and allows for clear and rational long-term planning and investment in the community.

As part of the planning process, a range of revitalization tools and incentives were explored. An analysis of these tools and incentives that will best suit the social, economic and environmental context of downtown Aldergrove should be conducted to facilitate re-development of the area. These include:

- regulatory tools that can be used to incent financial and cost efficiencies for redevelopment,
- market transformation financial incentive tools,
- municipal development finance tools for infrastructure and servicing upgrades, and
- governance models available for municipalities and development partners to further revitalization.

## **4.2 Servicing and Infrastructure**

A number of servicing and infrastructure upgrades will be required as part of implementation of the Aldergrove Core Area Land Use Plan. Additional studies will need to be undertaken to identify sufficient servicing and infrastructure to support the goals and policies in the Plan. These will include, but not necessarily be limited to:

- a parking and traffic study for the downtown area to analyze key issues (within the larger context of the entire community), such as:
  - alignments of pedestrian/cyclist shared use trails and greenways,
  - design standards of Bertrand Mews and their relationship to the larger network,
  - detailed design for the intersections on Fraser Highway, and
  - detailed designs for the Transit Plaza on 272 Street;
- a detailed analysis of capacity requirements and phasing for water, sanitary sewer and storm water management; and
- a district energy study.

## **4.3 Habitat Stewardship**

A habitat protection and enhancement plan will be required for the reach of Bertrand Creek in the plan area. The habitat protection and enhancement plan should be consistent with the Township Streamside Protection Bylaw, senior government regulation and policy for the protection of fish habitat, the Bertrand Creek Integrated Stormwater Management Plan (2009), and the Bertrand Creek Enhancement Project Final Report (1996). The plan will include a decision making structure to ensure consistent interpretation and implementation of the Streamside Protection Bylaw.

## **4.4 Development Permit Area**

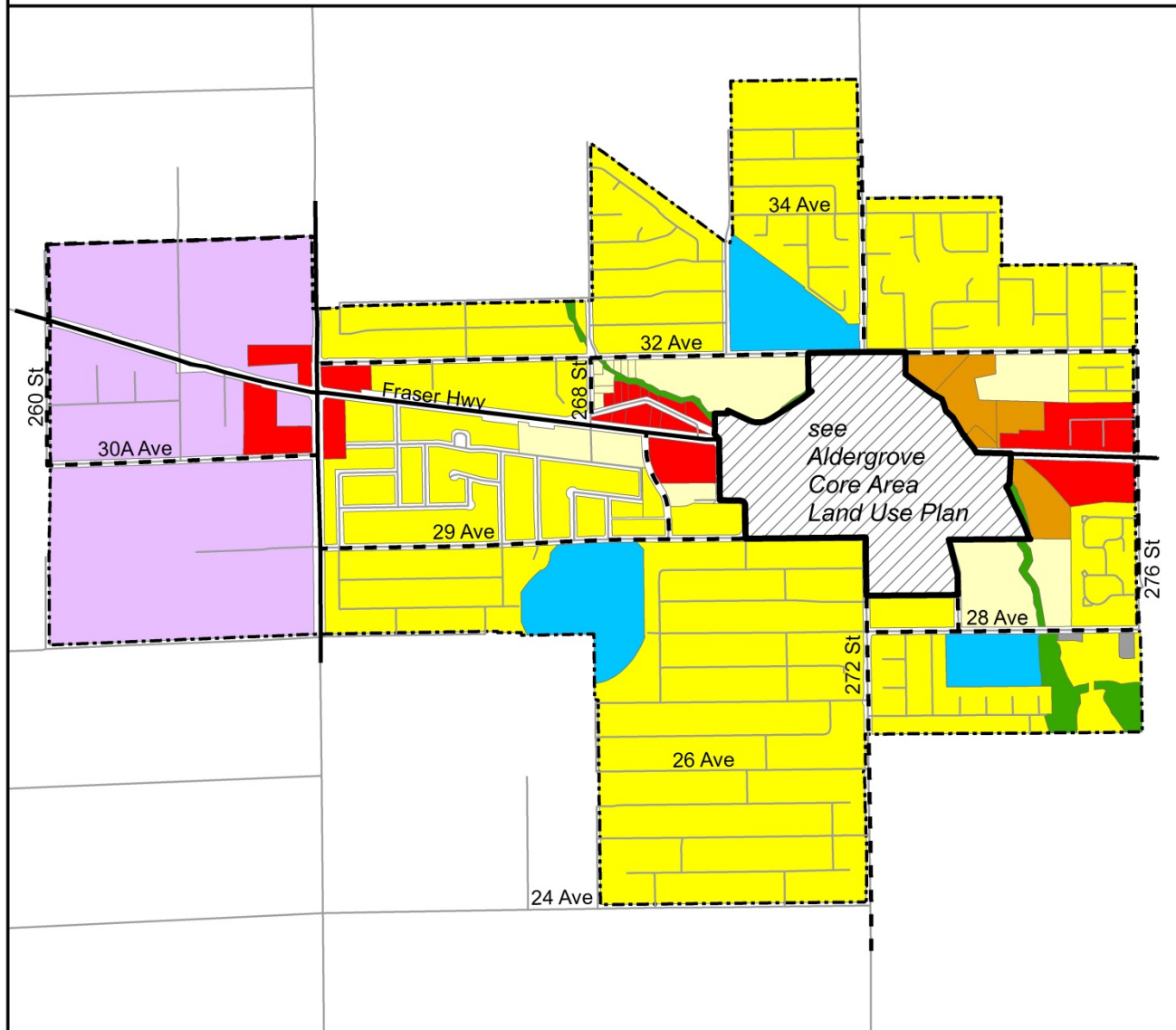
The areas shown as Development Permit Area “D” in Schedule “B” Aldergrove Development Permit Areas are designated development permit areas under Section 919.1 (1) (f) of the Local Government Act to establish objectives and provide guidelines for the form and character of development.

The objective of this development permit area designation is to ensure attractive residential mixed use and commercial development in the downtown core, based on an overall design concept for the area. The Aldergrove Core Area Concept Plan and Development Permit Guidelines can be found in Appendix “A” of the plan.

## **4.5 Prevalance of the Aldergrove Core Area Plan**

While section 4.2.1 of the Township of Langley Official Community Plan (OCP) Bylaw 1979 No. 1842 provides objectives, locational policies, implementation policies and development permit guidelines for high density residential development, developments in Development Permit Area “D” are subject to the policies and design guidelines in the Aldergrove Core Area Plan. Where a discrepancy exists between the OCP and the Aldergrove Core Area Plan, the implementation policies and development permit guidelines in the Aldergrove Core Area Plan shall prevail.

# Schedule "A" Land Use Concept



## LEGEND

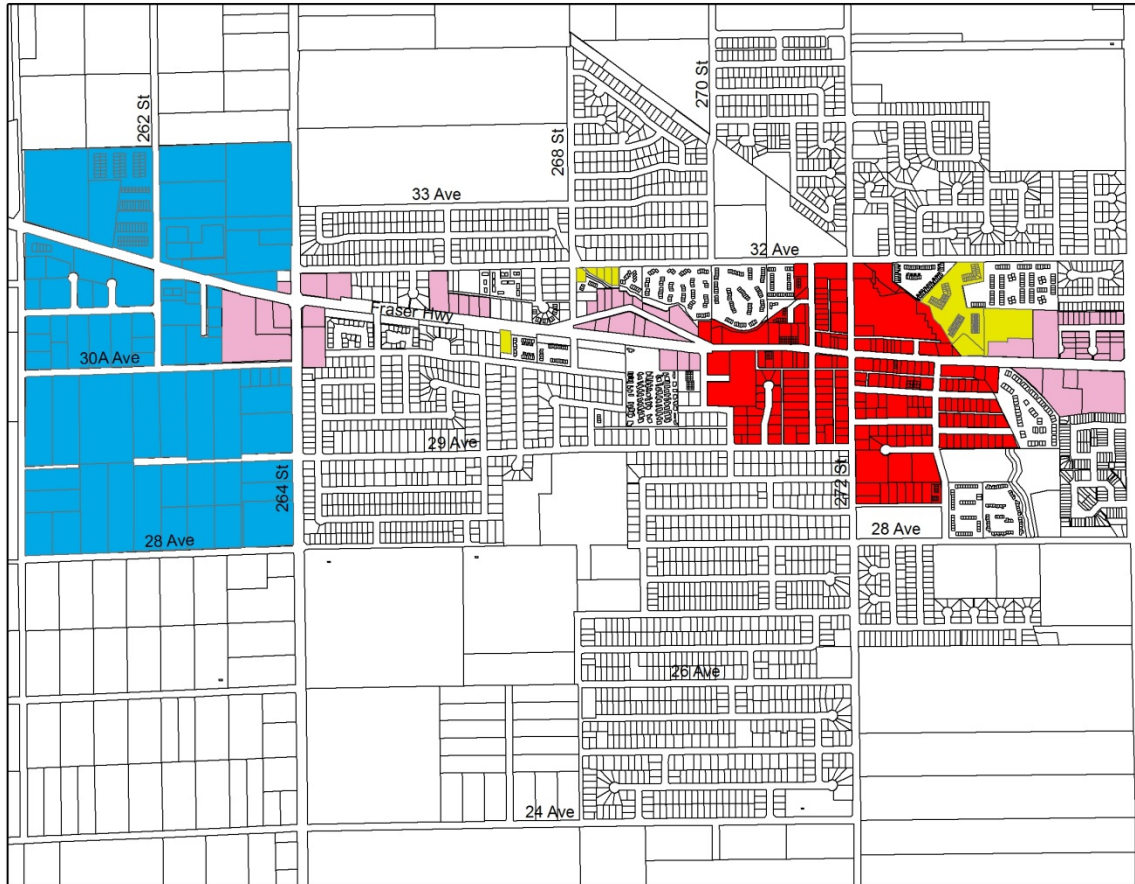


Low Density Residential	Conservation Area	Plan Boundary
Medium Density Residential	Commercial	Major Collector Road
High Density Residential	Industrial	Minor Collector Road
Recreation/Education	Public Utility	

Updated: March 21, 2016 - Bylaw No. 5126

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## Schedule "B" Aldergrove Development Permit Areas



### Legend

- Development Permit Area 'A' - Multi Family
- Development Permit Area 'B' - Commercial
- Development Permit Area 'C' - Industrial
- Development Permit Area 'D' - Downtown Core Area  
(see Aldergrove Core Area Land Use Plan and Design Guidelines)

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### Development Permit Areas

## Aldergrove Core Area Concept Plan and Development Permit Area Design Guidelines

(for Development Permit Area "D" - Downtown Core Area)



Figure 1: Aldergrove Core Development Permit Area



Figure 2: Aldergrove Core Area Concept Plan



## 1.0 INTRODUCTION

### 1.1 Designation and Justification

The Local Government Act Section 919.1 (1) states that an official community plan may designate development permit areas for the establishment of objectives for the form and character of commercial, industrial and multi-family residential development (Section 919.1 (1) (f)). Section 919.1 (2) states that with respect to areas designated under subsection (1), the official community plan must:

- (a) describe special conditions or objectives that justify the designation, and
- (b) specify guidelines respecting the manner by which the special conditions or objectives will be addressed.

The Aldergrove Core Area as identified in Figure 1 is designated as a development permit area under section 919.1 (1) of the Local Government Act to guide the form and character of multi-family, and commercial development.

### 1.2 Purpose and Intent

The Aldergrove Core Area Plan (ACAP) provides a comprehensive planning framework to accommodate an anticipated future downtown population of 5000 residents through the creation of a highly sustainable and livable town centre. The Concept Plan described in Section 2.0, and the Development Permit Design Guidelines in Section 3.0, establish the community's aspirations regarding the form and character of public and private realm development and overall implementation of the plan. The specific urban design objectives for downtown Aldergrove are established in Section 3.1: Intent and Objectives.

It is important to note that the concept plan is not a master or official development plan. Rather, it is a conceptual drawing whose purpose is to illustrate the urban design concepts, strategies and guidelines that form the basis for implementation of the urban design and planning vision for the downtown as described in the ACAP.



The Aldergrove core area charrette team

## Concept Plan Overview



**Aldergrove Transit Plaza**



**Bertrand Mews**



**Active retail frontage along Fraser Highway and 272 Street**



**Downtown Residential Street**



**Downtown Greenway**



**Enhanced setbacks and ecological restoration along Bertrand Creek**

## 2.0 THE CONCEPT PLAN

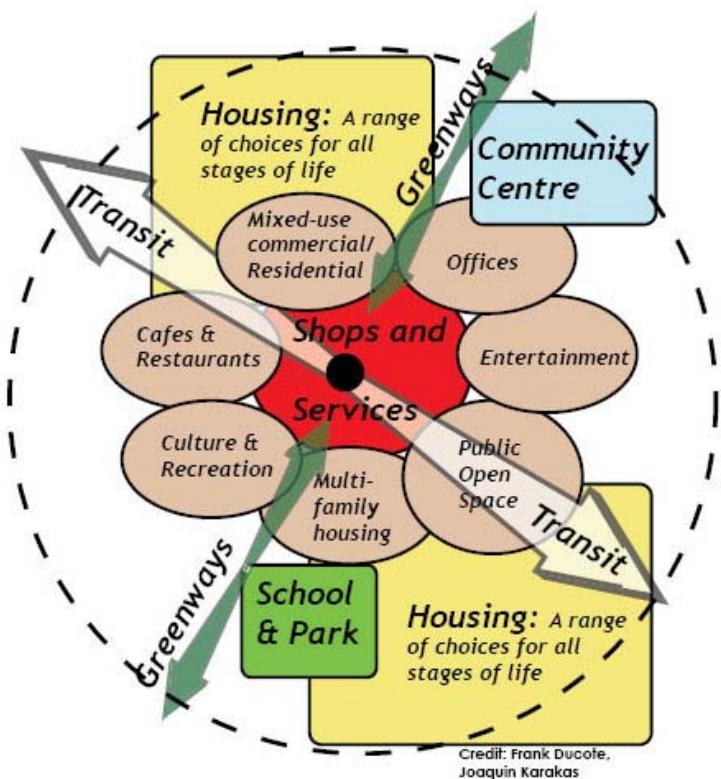
The concept represents a vision to transform downtown Aldergrove into a compact, mixed-use, highly walkable and transit oriented town centre. The land use framework creates significant additional residential capacity and a mix of housing types through the introduction of medium and higher-density residential development in both ground-oriented housing and apartments. The concept is also premised on maintaining and enhancing the Fraser Highway and 272 Street as vibrant commercial streets, with a mix of shops, services, cafes and restaurants to serve and provide jobs for local and regional residents. While the concept includes tall buildings, a human scale design is emphasized through pedestrian uses, comfort and safety.

### 2.1 Overview

The plan emphasizes the creation of great streets and public open spaces through a high quality of design and a positive interface between the public and private realm at the street level. Pedestrian oriented and humanly scaled streets and plazas, and a highly connected pedestrian network, are other essential elements of the plan which, together, are intended to foster a functional, aesthetic and vibrant downtown Aldergrove. A reference to downtown Aldergrove's past is achieved through a modern interpretation of historic form and character elements illustrated in a set of urban design guidelines intended to foster creative and innovative design responses to sustainability, livability, and sense of place, and through public art referencing Aldergrove's agricultural roots. Other important plan elements are:

- **on and off-street greenways** that emphasize comfortable, safe and convenient pedestrian travel and activities within the downtown, and connect to a wealth of parks and other amenities located immediately adjacent to the downtown;
- **new street network connections** to create more route options for pedestrians;
- **Bertrand Mews**, a new small scale, one-way local street running parallel to Bertrand Creek with primarily residential frontage and extensive traffic calming measures;
- **mid-block paseos or pass throughs** to create pedestrian connections to main commercial streets and through the downtown;
- **a new civic/recreational precinct** located on the Aldergrove Elementary School site that includes a pool and ice rink;
- **a transit plaza** located adjacent to the main bus exchange, lined with shops, cafes and other active uses, containing pedestrian amenities, public art, and able to accommodate outdoor community events including a regular farmers market;
- **pedestrian crossings** at key locations throughout the downtown; and
- **urban parks and plazas** located throughout the downtown.

Downtown Aldergrove – Conceptual Planning Framework



## 2.2 Commercial Continuity: Active Retail Frontage



Extent of continuous commercial frontage desired



Active uses front onto, and can even spill out onto, public streets and open spaces



## 2.3 Residential Streets



Residential buildings present a friendly, welcoming face to the street by minimizing the distance they are set back from the sidewalk and by incorporating ground floor entrances, patios and upper storey balconies that overlook the street, enhancing pedestrian activity and safety



## 2.4 Off-street Greenways



**Greenways incorporate integrated stormwater source controls**

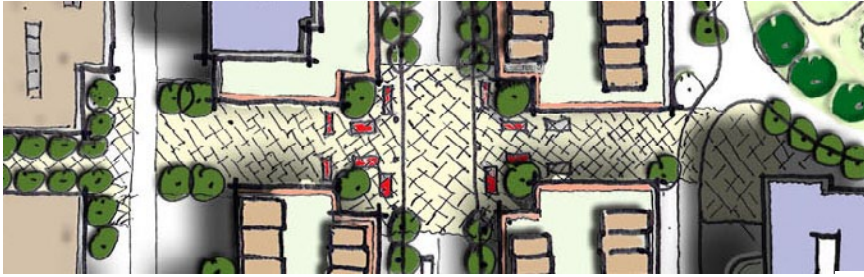


**Greenways are a significant and functional component of the downtown's overall mobility network**



**Residential buildings oriented positively towards greenways, with entrances, patios and upper storey balconies overlooking the greenway to enhance pedestrian activity and safety**

## 2.5 Downtown Parks and Plazas



Active uses spill out onto plaza



Aldergrove transit plaza is a key activity node located in the heart of the downtown, framed and defined by adjacent buildings and active uses



Public art incorporating water feature



Adjacent buildings and uses front onto, activate and define plaza

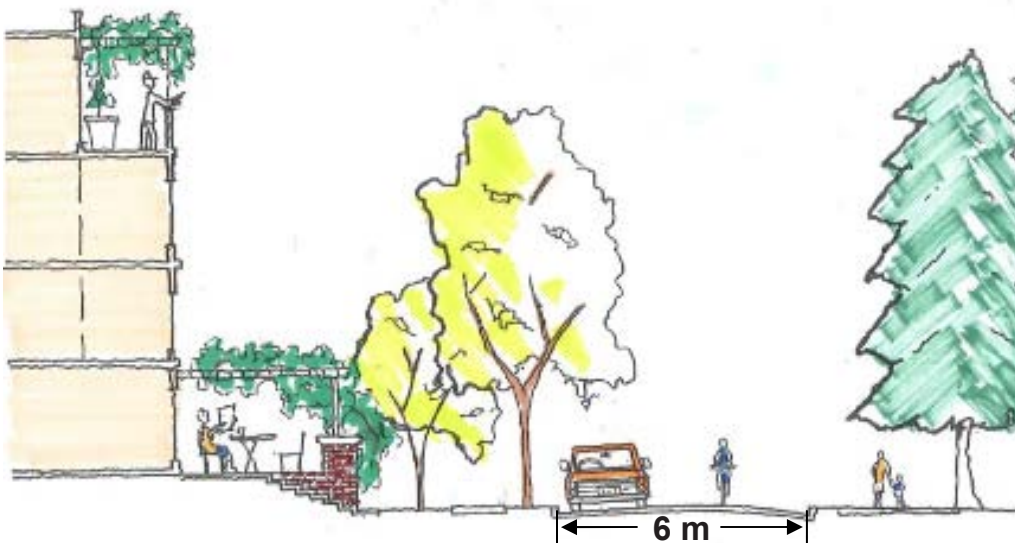


Small urban parks with formal features and passive programming

## 2.6 Bertrand Mews



**Bertrand Mews is a proposed one way local street that runs along and is significantly set back from Bertrand Creek with residential frontage and adjacent pedestrian trails**



## 2.7 Massing Model: Build Out Capacity – to 2041 and Beyond

The massing model illustrates a full build out scenario of the proposed land use policies and urban design strategies for the downtown. It is not a development plan or a master plan. Rather, it is an illustration of both the development capacity provided by the plan and the overall form and structure as guided by the concept plan and design guidelines.



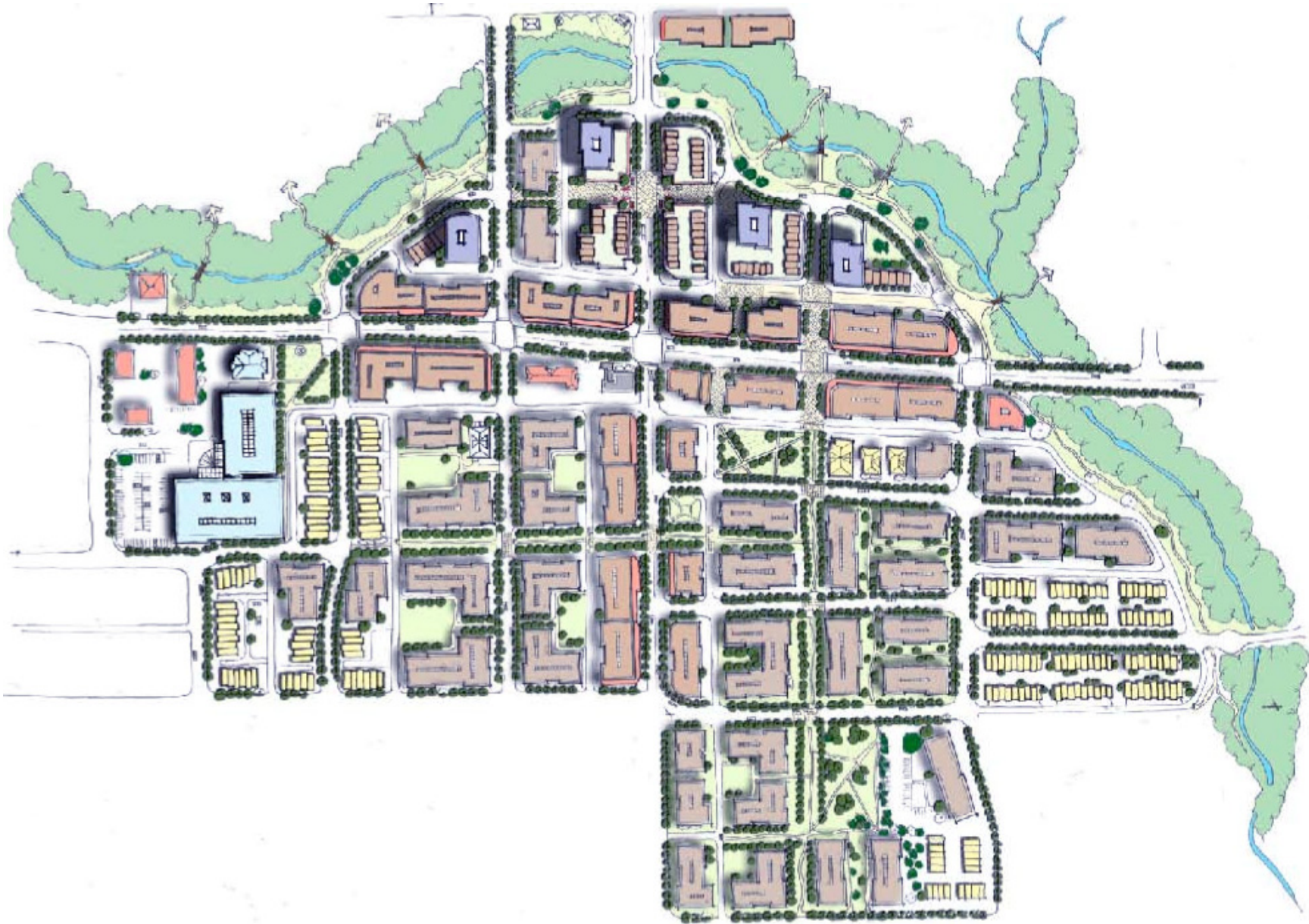
**Tall buildings located adjacent to Bertrand Creek**

**Townhouses are located towards the edge of the plan area to achieve a sensitive transition in scale to adjacent, less intensive uses**

**A continuous planting of trees along streets and greenways helps create spatial definition and a sense of enclosure throughout the downtown**

**Four to six storey residential buildings are the predominant form of proposed future development**

The Aldergrove Core Area Illustrative Concept Plan



## 3.0 DESIGN GUIDELINES

### 3.1 Intent and Objectives

Urban design is the comprehensive and cohesive combination of buildings, streets, and open space which has, as its objective, the creation of memorable public space. The essence of good urbanism is determined by the relationship between the public and private realm at the street level. Pedestrian oriented and humanly scaled streets, squares, and plazas are essential elements to the making of a functional, aesthetic and vibrant place.

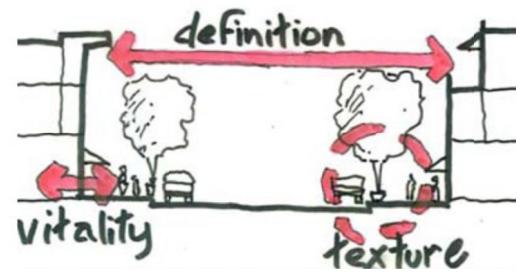
In order to support vibrant active streets and a sense of community, the following three fundamental approaches shall guide private development in the downtown:

- 1) Build to the sidewalk.
- 2) Make the street front visually and physically permeable (transparent).
- 3) Locate off-street parking behind or under the building.

#### Great Streets

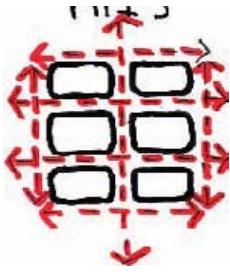
The design guidelines are premised on the creation of great streets as described by the general objectives and approaches below. As such, these provide an overview of the following guidelines (sections 3.2 – 3.13)

- Streets are places for people to walk leisurely or with a purpose. Streets should have a pleasing experiential and social dimension;
- Building frontages along urban streets and public open spaces are the strongest determinants of character and quality. Continuous frontages create interesting and attractive walking environments while providing friendly and animated streetscapes;
- Streets should provide physical comfort. A sense of physical comfort derives from both a perceived sense of security and the design elements of the street;
- Streets require definition. Streets should have clear boundaries, or 'walls', that define an outdoor room. Both buildings and trees can provide a sense of definition or enclosure;

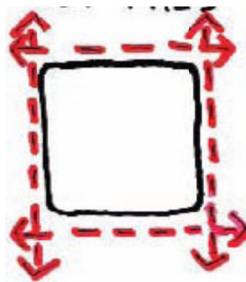


- Streets require qualities that engage the eye. Visual interest at the human scale adds to the experiential dimension of the street;
- Streets require transparency. Windows at eye level are inviting, engaging, sociable, and provide for natural surveillance;
- Streets must serve adjacent land uses in an appropriate manner; and
- Streets must accommodate emergency, transit, and service vehicles.

**THIS**



**NOT THIS**

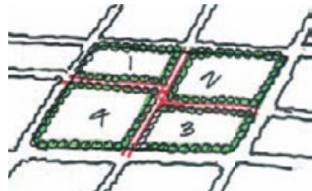
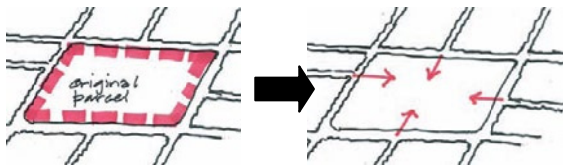


### 3.2 Connectivity: Enhancing the Pedestrian Network

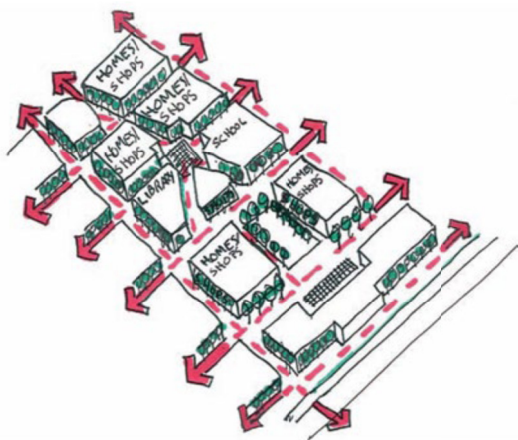
**Intent:** To encourage new development or re-development on large sites to incorporate new streets, greenways and pathways that extend the existing surrounding network and increase direct connections for pedestrians and cyclists.

#### Guidelines

- Permeate large blocks with new streets and greenways that connect with the existing network of streets and pedestrian pathways.
- Avoid cul-de-sacs and other physical barriers which deter or prevent people from walking or cycling through the downtown and remove these as opportunities arise.
- Where large blocks are unavoidable, provide publicly accessible mid-block pedestrian and bicycle pathways that connect with the existing, surrounding network.
- Increase pedestrian connections across Bertrand Creek to create more options for pedestrians and cyclists traveling to and from the downtown.



**Large blocks should be broken up to create smaller ones as re-development opportunities arise**



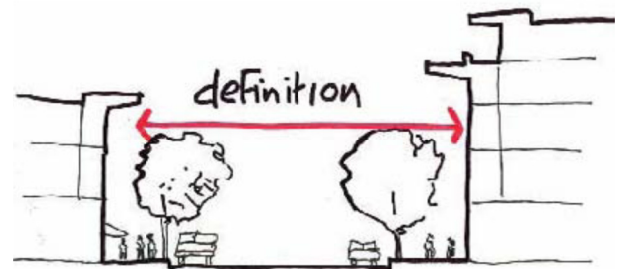
**Greenway increases connectivity of pedestrian network**

### 3.3 Street Definition

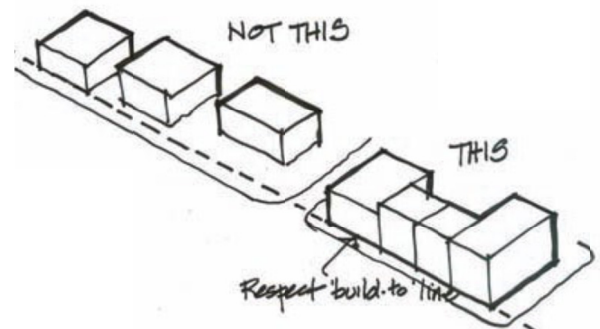
**Intent:** To site and design buildings to positively frame and define streets and other public open spaces.

#### Guidelines

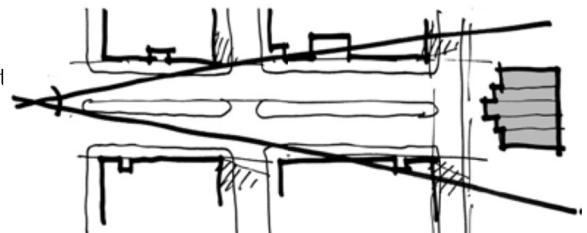
- Minimize the distance between the building and the sidewalk to create good street definition and a sense of enclosure.
- Generally, ground floor commercial uses should be built to the front property line so that a continuous street frontage and street definition is maintained.  
A small setback is encouraged where a pedestrian courtyard or other similar features benefiting pedestrian character are provided, to respond to the building setback from an adjacent property and to allow for projecting balconies and bay windows while not encroaching on public property.
- Site and design buildings to respond to specific site conditions and opportunities such as oddly shaped lots, location on prominent intersections, sites framing important open spaces, corner lots, sites with buildings that terminate a street end view, and views of natural features.
- A minimum 3 storey street-wall and a maximum 4 storey street-wall should be maintained throughout the downtown except along the Fraser Highway and 272 Street (See scale and massing guidelines in section 3.6)



**Buildings frame and define streets, creating a human scale public realm**



**Site buildings continuously using a common 'build to' line**



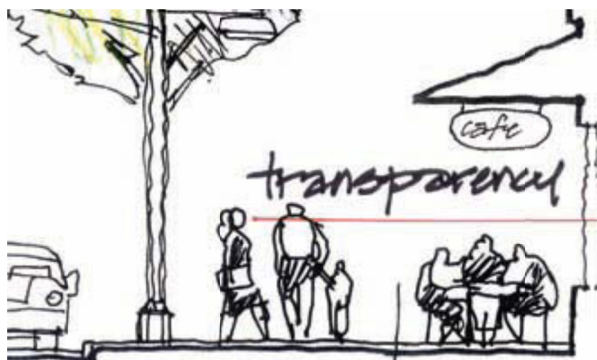
**Siting and design responds to location on intersection**



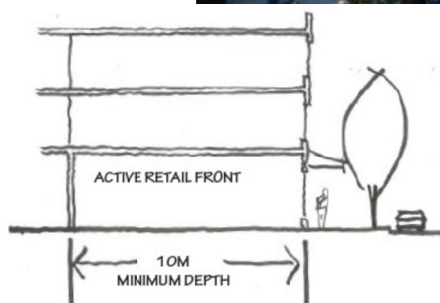
**Provide distinctive architectural features for buildings that terminate a street end view**



**Orient buildings to, and provide direct pedestrian access from, the adjacent public street/sidewalk**



**Transparency creates visual interest, activity and safety**



### 3.4 Active Frontages: Transparency

**Intent:** To ensure buildings are sited and designed to create welcoming frontages and encourage street vitality, visual interest and safety.

#### Guidelines

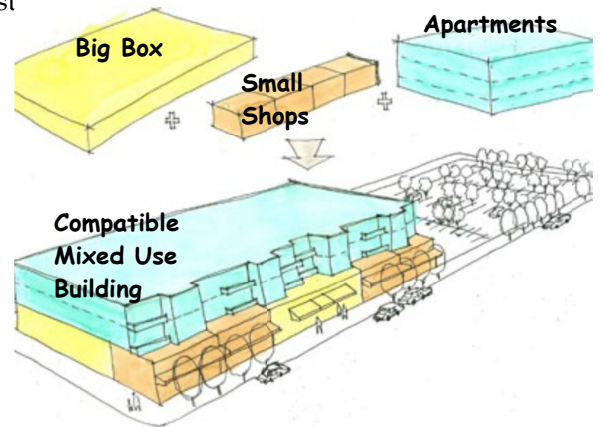
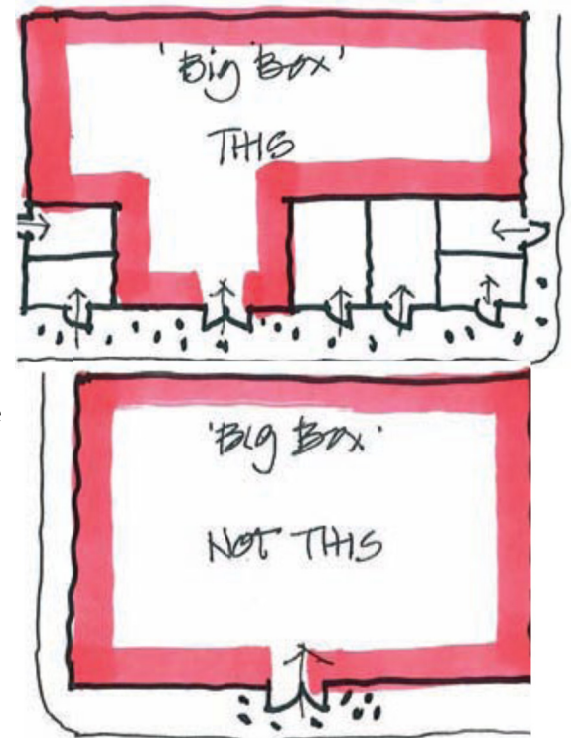
##### Commercial and Mixed Use Buildings

- the established orthogonal alignment of building face to the street is an ordering principle in the downtown. Therefore, provide pedestrian access to storefronts and businesses from the adjacent public street. Orient upper storey windows and balconies to overlook adjoining public streets, squares, greenways and other public open spaces.
- On corner sites, develop both street facing facades as front elevations with pronounced entrances oriented to the corner and/or primary streets.
- Shops should have a minimum depth of 10 metres.
- Ensure shop fronts are transparent. Transparency occurs where public and private realms meet. Good transparency is when interior (private) uses are visible from, and can even spill out onto, the (public) sidewalk or plaza, and further, when the use of public space is visible from inside buildings to allow for casual surveillance. Use of figured or frosted glass is strongly discouraged and is to be avoided.
- Recess entrances to buildings from the sidewalk or property line a minimum of 1.2 m in order to provide for door swings and for weather protection.
- Incorporate frequent entrances into commercial frontages along the street. A maximum spacing dimension of 8 m along Fraser Highway and 12 m along other commercial streets in the downtown is preferred.



**A maximum 8 metre frontage module is preferred along the Fraser Highway to create human scale and to reinforce its function as Aldergrove's main street**

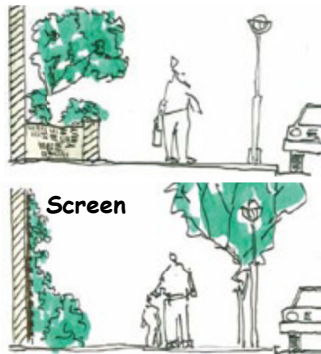
- g) Large floor plate commercial developments will respond to the prevailing street character in the downtown by incorporating frequent entrances and transparent shop front windows consistent with these guidelines. Large format commercial buildings with compatible uses should incorporate smaller shops wrapped around its edges and residential uses above to better integrate these buildings and uses and make them more compatible with the existing character of downtown streets.
- h) Avoid expansive blank walls (i.e., over 5 m in length) and retaining walls adjacent to public streets. When blank walls are unavoidable, they shall be mitigated using appropriate design treatments which can include, but are not limited to, the following:
- Installing a vertical trellis in front of the wall with climbing vines or plant materials;
  - Setting the wall back to provide room for evergreen and conifers to provide year round screening;
  - Providing art (such as a mosaic, mural, relief, etc.) over a substantial portion of the wall surface;
  - Employing quality materials and/or different texture and colours to make it visually interesting;
  - Providing special lighting, a canopy, awning, horizontal trellis or other pedestrian oriented features that break up the size of the blank wall's surface and add visual interest and
  - Incorporating them into a patio or sidewalk café.



**NOT THIS**



**THIS**



When unavoidable, blank walls should be screened with landscaping or incorporate a patio café or special materials to make it visually more interesting



Large format commercial uses incorporated into a mixed use building with small shops fronting onto the street



### Residential Buildings/Frontage

- i) Site and orient multi-family residential buildings to overlook public streets, parks, and walkways and private communal spaces while ensuring the security and privacy of its residents.
- j) Incorporate individual entrances to ground floor units in residential buildings that are accessible from the fronting street. This provides easy pedestrian connections to buildings, encourages street use and walking, and overall enhances social activity and safety.
- k) Set back residential buildings on the ground floor a minimum of 2 m and a maximum of 5 m to create a semi-private entry or transition zone to individual units and to allow for an elevated entryway or 'stoop'.
- l) Apartment lobbies and main building entries shall be clearly visible from the fronting street, and have direct sight lines into them. Where possible, apartment lobbies should have multiple access points to enhance building access and connectivity with adjacent open spaces.



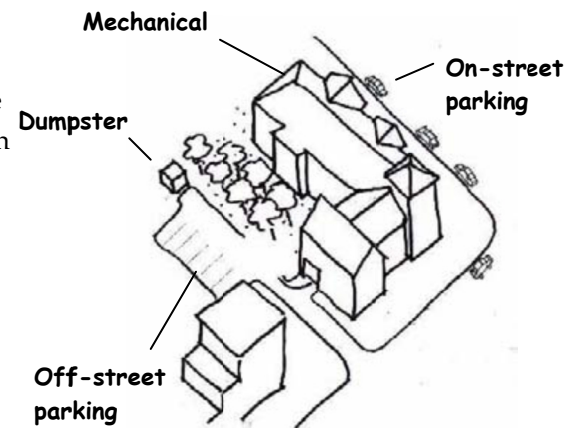
**Residential buildings with ground floor units addressing the street with balconies, entries and stoops directly accessible from the fronting street or open space**

### 3.5 Parking, Servicing and Access

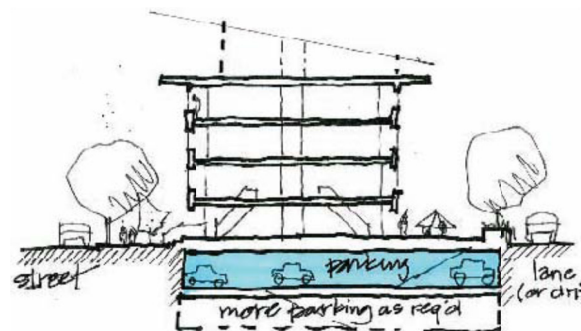
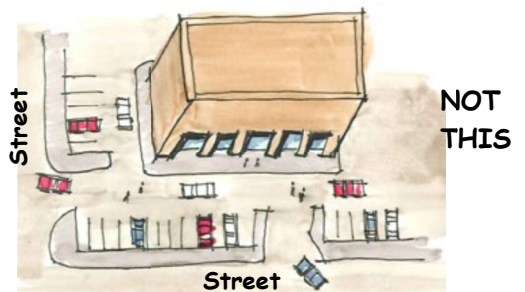
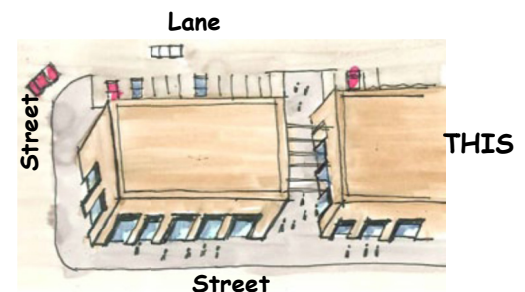
**Intent:** To ensure the provision of adequate servicing, vehicle access and parking in the downtown while minimizing negative impacts on the safety and attractiveness of the public/pedestrian realm.

#### Guidelines

- a) A welcoming pedestrian environment with continuous street edge definition is critical to the character of the downtown. Therefore, it is important that vehicular and service functions and other 'back of house' activities remain primarily on the lane where possible, so as not to conflict with pedestrian oriented street activity, particularly on downtown commercial streets.
- b) Structured underground or 'tuck-under' parking is preferred over off-street surface parking.
- c) Where off-street surface parking is unavoidable, it should be located to the rear of the building with parking access from the lane or side-street.
- d) Locate public on-street parking at the curb to provide convenient and direct access to commercial and residential entrances along the street.
- e) Off-street parking shall not be located between the front face of a building and the public sidewalk.
- f) If located beside the building and adjacent to the public sidewalk, screen surface parking areas from sidewalks and other active open spaces using materials that provide a visual buffer while still allowing clear visibility into the parking areas to promote personal safety and security. Screening could include landscaping, a trellis, or grillwork with climbing vines.
- g) In general, vehicular access should be from the lane. Where there is no lane, and where the re-introduction of a lane is difficult or not possible, access may be provided from the street, provided:
  - Access is from the long face of the block;
  - There is minimal interruption of the pedestrian realm and streetscape treatment;
  - Waiting, or pick-up drop-off areas are located internal to the site, not in the public right-of-way; and
  - There is no more than one interruption per block face and only one curb cut on the street.



Off-street parking and other 'back-of-house' uses shall be located behind, underneath (preferred) or beside (not desired, but acceptable), but never between the front of a building and the public sidewalk



Off-street parking and servicing located to minimize impacts on pedestrian realm

- h) Any vehicular entrance and its associated components (doorways, ramps, etc.), whether from the street or lane, should be architecturally integrated into the building so as to minimize its exposure. In particular, avoid ramps located directly off the street or lane. Minimize negative impacts of parking ramps and entrances through treatment such as enclosure, screening, high quality finishes, sensitive lighting, and landscaping.
- i) Vehicular entrances and curb cuts are not permitted along the Fraser Highway within the downtown plan area.
- j) Clear lines of sight should be provided at access points to parking, site servicing, and utility areas to enable casual surveillance and safety.
- k) Share parking and access is encouraged where possible.



**Access to underground parking is architecturally integrated to minimize impacts on the pedestrian realm**



**On street parking at the curb should be provided throughout the downtown to provide direct access from the street to businesses and homes**

### 3.6 Scale and Massing

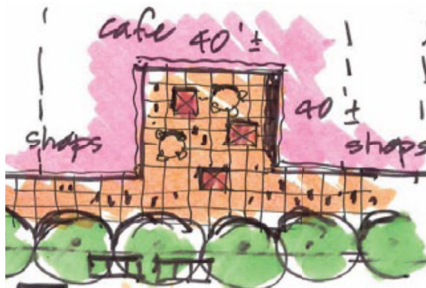
**Intent:** To reduce the visual mass of large buildings and ensure a sensitive transition to adjacent, less-intensive areas.

#### Guidelines

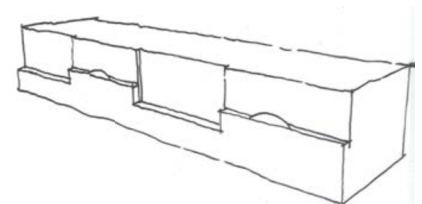
- Break up the visual mass of large buildings by incorporating minor visual breaks in their facades using vertical setbacks and upper storey step-backs (façade modulation and articulation). Commercial/mixed use building facades should be articulated into modules that are a maximum of 15 m in length, and residential buildings a maximum 30 m.
- Buildings over three storeys in height shall be a maximum of 60 m in length. Buildings three storeys in height and less shall be a maximum of 40 m in length. This can be achieved by incorporating a substantial setback such as a courtyard or framed periodic openings to provide public views into private open space features.
- Buildings up to four storeys in height should step the fourth storey back a minimum of 1.5 m.
- Buildings between four and six storeys in height should step back the top two storeys a minimum of 1.5 m.
- A minimum three storey street wall should be maintained for buildings in the downtown, except along the Fraser Highway and 272 Street, where a maximum one and/or two storey street-wall is desired to reflect the traditional façade pattern. An active skyline incorporating contemporary interpretations of traditional features such as false fronts may be incorporated into the roof line at the building setback as a reference to the traditional architectural form and character of these streets. However, a faux pioneer look or theme is strongly discouraged and should be avoided (see also architectural concept guidelines in section 3.9).



A courtyard can be used to break up the visual mass of large buildings  
Page | 69



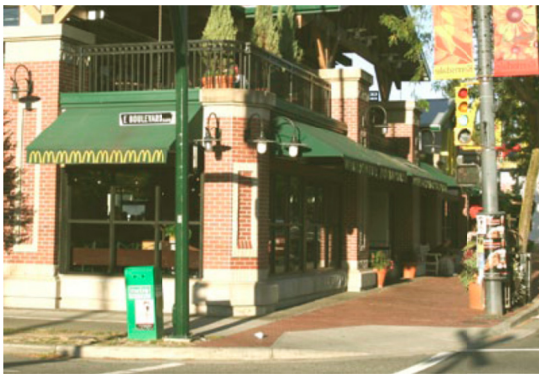
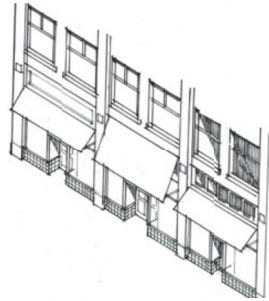
Vertical setbacks and upper storey step-backs break up the visual mass of these buildings



Buildings along the Fraser Highway should step back after the first or second storey to reflect the traditional commercial building heights in the downtown



The placement of awnings and canopies should reflect the building façade's articulation and fenestration pattern



Weather protection in the form of awnings or canopies contributes to the character and pedestrian comfort of streets



### 3.7 Weather Protection

**Intent:** To provide comfort for pedestrians and emphasize the pedestrian orientation and function of public streets through the provision of weather protection while enhancing the overall appearance and character of commercial areas in the downtown.

#### Overview

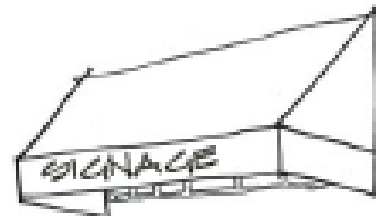
Awnings and canopies are the preferred measures for providing necessary weather protection as part of private development along public streets, or on privately-owned property. Awnings are a light, detachable structure of fabric or other flexible material supported from the building by a fixed or, preferably, a retractable frame to provide shelter from sun, rain and snow. Canopies are rigid structures extending out from the building face and entirely supported from the building. Awnings are preferred along the Fraser highway, while awnings and/or canopies are acceptable for other commercial areas in the downtown.

#### Guidelines

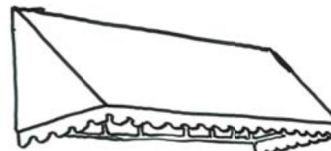
- Provide weather protection along all commercial streets and plazas. Enhanced weather protection is encouraged at the entrances of major buildings, adjacent to bus zones and street corners where people wait for traffic lights, over store fronts and display windows, and any other areas where significant waiting or browsing by people occurs.
- Awnings, canopies and overhangs should be architecturally integrated and incorporate architectural design features of buildings from which they are supported.
- Place awnings and canopies to reflect the building's architecture and fenestration pattern (placement of windows and doors).
- Placement of awnings and canopies should balance weather protection with daylight penetration. Avoid continuous opaque (solid) canopies that run the full length of facades.
- Along the Fraser Highway, weather protection should be in the form of awnings.
- Awnings and/or canopies can be used along other commercial streets in the downtown.

## Awnings

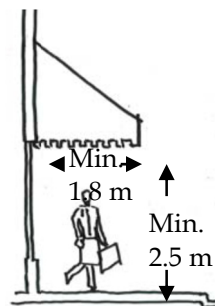
- g) Awnings should have a minimum vertical clearance of 2.5 m measured from the sidewalk and shall extend out over the sidewalk a minimum of 1.8 m, with greater coverage desirable in areas of high pedestrian traffic and where sidewalk widths are adequate. Awnings should not occupy more than 2/3 of the total sidewalk width.
- h) Awnings shall have a minimum slope of 30 degrees to allow for proper drainage and self cleaning action of rain and wind.
- i) Along the Fraser Highway, awning design should reflect a traditional style. 3 or 4-point awnings are preferred. The use of quarter barrel awnings are strongly discouraged and should be avoided.
- j) Construct awnings of durable, colour-fast material. This may include reinforced plastic coated fabric provided the look and feel of canvas is maintained.
- k) Signage on the front face of a 4 point awning shall not exceed 0.3 m in height.



**4-point awning with compatible signage**

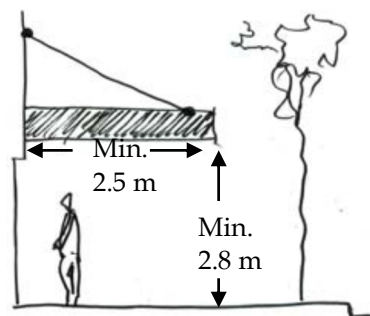


**3-point awning with valance**



## Canopies

- l) Canopies shall have a minimum vertical clearance of 2.8 m measured from the sidewalk and should extend out over the sidewalk at least 2.5 m while maintaining a minimum 0.6 m setback from the outer face of the curb.
- m) Use transparent and translucent canopies to allow natural light to penetrate to storefronts and the sidewalk.
- n) Steel and glass are the preferred materials for canopies.
- o) Design canopies extending over building frontages greater than 30 m to reduce their apparent scale and length, so as to relate better to the pedestrian scale of the street. The preferred approach is to break up the canopy to reflect the building façade's architecture and fenestration pattern.
- p) Construct canopies of non-combustible material and support them entirely from the building. Post supports for canopies within Township property are not permitted.
- q) Incorporate architectural glare-free lighting into the canopy soffit that has either a low level light source or one not directly visible to pedestrians. Fluorescent tube lights are not permitted for this use.



**Transparent canopies made of glass and steel are preferred over solid wood, opaque canopies**



**THIS**



**Flush mounted signs, and blade signs hanging from weather protection, are preferred in downtown pedestrian areas**



**NOT THIS**



**Internally lit and plastic box signs, and large signage on awnings are discouraged and should be avoided.**

### 3.8 Signage and Lighting

**Intent and Overview:** Signage and lighting play a significant role in the overall quality and character of shopping areas in the downtown. Integrated building lighting can make a positive contribution to the sense of safety and security pedestrians experience in the downtown through a combination of street, pedestrian and architectural lighting. A signage and lighting program for any commercial development should be designed as a totality, with signs, lighting and weather protection architecturally integrated from the outset.

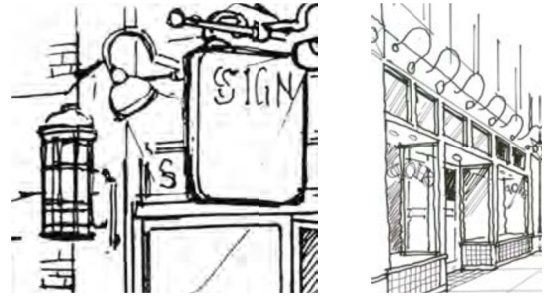
#### Guidelines

##### Signage

- Limit signage in number, location and size to reduce visual clutter and make individual signs easier to see.
- Provide visible signage identifying building address at all entrances.
- Provide attractive signage on commercial buildings that identifies uses and shops clearly but which is scaled to the pedestrian rather than the motorist.
- Visual or representational and iconic signs are encouraged to supplement conventional textual signs to help establish a special character to the downtown.
- A single external sign band may be applied to each façade at the first storey, and may not exceed 0.9 m in height along any length.
- Signage shall be externally lit. Signage within shop front glazing may be back lit, but shall not exceed 0.5 m in height and 2 metres in length.
- Signage within residential frontages shall not exceed 0.1 square m.
- The following are preferred or acceptable types of commercial signage in the downtown:
  - Projecting two-dimensional or 'blade' type signs suspended from canopies and awnings (fitting within a 92 cm x 153 cm (36" x 60") horizontal rectangle):
  - Flush mounted fascia signs;
  - Externally lighted signs;
  - Vertical banners; and
  - Individual cut-out or silhouette letter signs mounted on storefronts. Individual letters should not exceed 45 cm (18") in any dimension.
- The following types of signage are strongly discouraged and should be avoided:
  - Signs as awnings/awnings as signs;
  - Internally lighted plastic box signs;
  - Pylon (stand alone) signs;
  - Rooftop signs; and
  - Vertical projecting signs

## Lighting

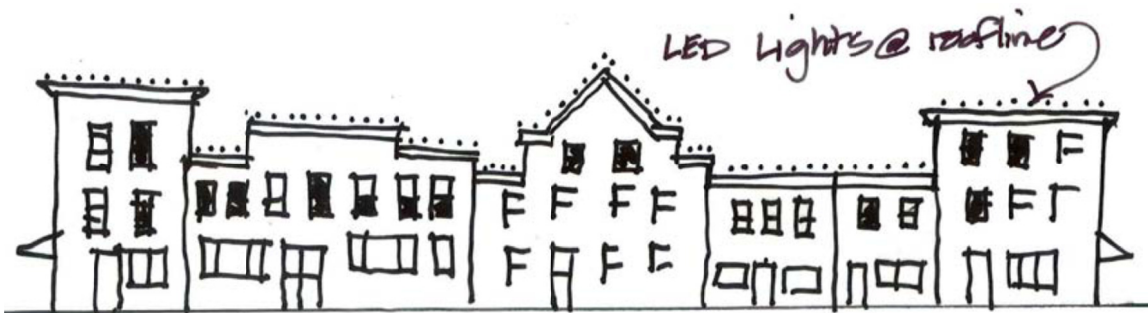
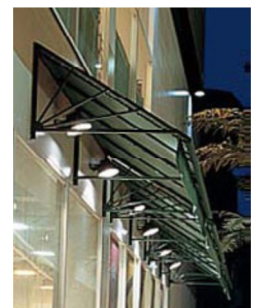
- j) Illuminate building facades and features by providing architectural lighting on the face of commercial buildings and at main entries to multi-family residential buildings to help create a sense of safe and intimate space around a building.
- k) Paths and entry areas shall be sufficiently lit to ensure pedestrian comfort and security.
- l) Provide pedestrian scaled lighting with a high quality of design detail above sidewalks for night time visibility.
- m) Full-spectrum 'white' light or incandescent sources are preferred in public areas.
- n) Ensure lighting is sensitive to nearby residential uses. Avoid visible glaring light sources by using down-and/or up-lights with cutoff shields.
- o) Gooseneck lights and sconces applied to fascias are the preferred type of storefront lighting.
- p) Incorporate valance lighting into canopies and up lighting to illuminate pathways.
- q) LED lighting of storefronts and street trees is encouraged.
- r) Avoid the use of exterior fluorescent light sources.



Sconce and gooseneck lighting are preferred for commercial buildings



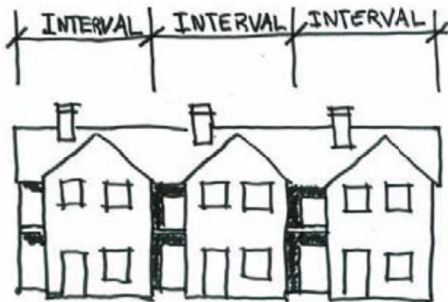
A combination of wall mounted lights, valance and up lighting animate this building façade and adjacent pedestrian areas



L.E.D. lighting along roof lines animates buildings at night



These buildings express a unified architectural concept that incorporates both variation and consistency in a pleasing composition



Breaking up a building's façade into a series of intervals creates a more pleasing and human scale expression

### 3.9 Architectural Concept: Achieving a Human Scale

**Intent and Overview:** The following architectural guidelines are not intended to be prescriptive but rather, to encourage flexibility and innovation in building design. They are focused on form over character, and in achieving a timeless architecture that emphasizes a human scale through the use of articulation, architectural details and features. A contemporary interpretation of traditional building forms and architectural features and details is encouraged to reference Aldergrove's small town, pioneer heritage. However, careful consideration must be taken to avoid a faux pioneer look or theme, which is strongly discouraged for Downtown Aldergrove.

*Human Scale* refers to the use of architectural features, details and site design elements that are human proportioned and clearly oriented towards pedestrian activity. A building has good human scale if its details, elements and materials allow people to feel comfortable using and approaching it.

*Building articulation* refers to the many street frontage design elements, both horizontal and vertical, that help create an interesting and welcoming streetscape. These include building materials, special ground-floor design treatments, façade modulation, corner treatments, building setbacks for upper stories, and façade elements such as window treatments, building entries, and other architectural details which help define the public realm as a welcoming place.

#### Guidelines

- a) The design of new buildings and renovated existing buildings should express a unified architectural concept that incorporates both variation and consistency in façade treatments by, for example, articulating buildings facades into a series of intervals. This can be achieved by:
  - Façade modulation – stepping back or extending forward a portion of the façade to create a series of intervals or breaks in the façade;
  - Repeating window patterns at intervals that correspond to extensions and step backs;
  - Providing a porch, patio, deck, or covered entry for each interval;
  - Providing a balcony or bay window for each interval; and
  - Changing the roof line by alternating dormers, stepped roofs, gables, or other roof elements to reinforce the modulation or articulation interval.

- b) Incorporate a range of architectural features and design details into building facades that are rich and varied in detail to create visual interest when approached by pedestrians.

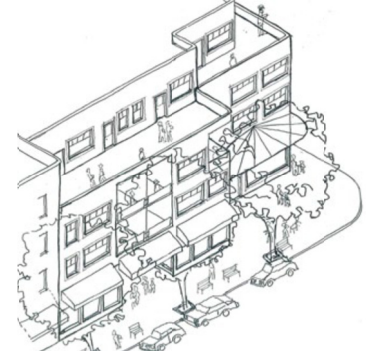
Examples of architectural features include:

- Building height, massing, articulation and modulation;
- Bay windows and balconies;
- Corner feature accents, such as turrets or cupolas;
- Decorative roof lines and cornices;
- Building entries; and
- Canopies and overhangs.

Examples of architectural details include:

- Treatment of masonry (such as ceramic tile, paving stones, brick patterns, etc.);
- Treatment of siding – for example the use of score lines, textures and different materials or patterning to distinguish between different floors;
- Articulation of columns and pilasters;
- Ornament or integrated art work;
- Integrated architectural lighting;
- Detailed grills and railings;
- Substantial trim details and moldings; and
- Trellises and arbors.

- c) When considering ornamental details, emphasis should be placed upon exterior wall materials, surface planes and textures, skyline articulation, and decorative motifs.
- d) Locate and design entrances to create building identity and to distinguish between individual ground floor units. Use a high level of architectural detail and, where appropriate, landscape treatment, to emphasize primary entrances, and to provide ‘punctuation’ in the overall streetscape treatment.
- e) Clearly distinguish the roof line of the building’s façade from its walls, for example, through the use of a cornice, projecting overhang, or decorative motif.
- f) Design balconies as integral parts of buildings. Use glazed or narrow metal spindle guardrails to maximize daylight penetration into dwellings.



**Architectural details and features help to create visual interest when approached by pedestrians**

g) Windows should be designed to reinforce human scale. This can be accomplished by incorporating individual windows in upper stories that:

- are vertically proportioned and approximately the size and proportion of a traditional window;
- include substantial trim or molding;
- are separated from adjacent windows by a vertical element;
- are made up of small panes of glass; and
- are separated and grouped together to form larger areas of glazing with moldings or jambs.



**Bands of windows broken up into vertical proportions help achieve a human scale**



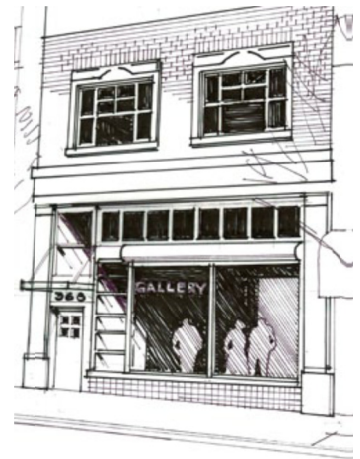
### Commercial Frontage

- h) Provide a continuous, transparent and welcoming retail frontage which will be a benefit to both the pedestrian and merchant.
- i) Establish a dominant frontage rhythm along the street through the use of vertical elements and a fenestration pattern (shop entrances and windows) arranged into modules. Frontage widths should be limited to 8 m along the Fraser Highway and fronting onto plazas and paseos. A maximum 12 m frontage should be maintained along other commercial frontage in the downtown.
- j) Shop fronts should incorporate the following design elements and details:

- A high percentage of glazing, in the display window area, transom windows and in the entry door(s);
- A recessed entry, with a rectangular or trapezoidal plan;
- Transom window above the entry, often stretching the full width of the shop front above the recessed entry;
- Paneled display windows;
- Weather protection, lighting and signage;
- A shop front cornice which is generally a reduced version of the main cornice atop the building; and
- A modern interpretation of a false front at the first step back for buildings along the Fraser Highway is acceptable although a faux pioneer look or theme is strongly discouraged and should be avoided;

- k) The lower façade of new buildings should be distinguished at its uppermost edge by a continuous intermediate cornice or similar decorative banding element.

**A contemporary interpretation (right) of a traditional shop front elements (above right)**



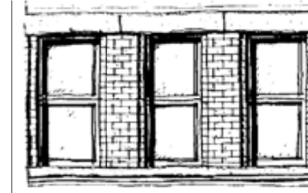
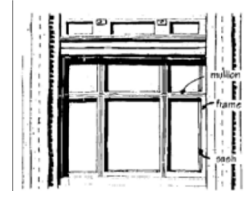
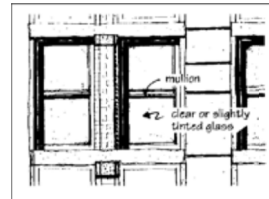
**Contemporary interpretations reference traditional architectural styles without being faux, tacky or kitschy**

l) The upper facades of buildings should:

- Be ordered by the use of vertical elements such as pilasters, columns, projecting bays and recessed balconies, and;
- Incorporate decorative roof lines and cornices to define the upper edge of the façade where the building steps back and at the top elevation of the building.

m) Windows incorporated into upper facades should:

- Be punched (recessed) a minimum 10 cm and incorporate lintels and sills;
- Be oriented vertically and organized into repetitive groups in relationship to the vertical elements which frame and divide the façade such as shop front modules, pilasters, columns and vertical step backs;
- Be built of wood, and be painted, stained, or clad; and
- Incorporate substantial trim and moldings.

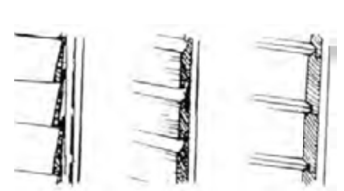
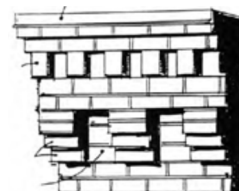
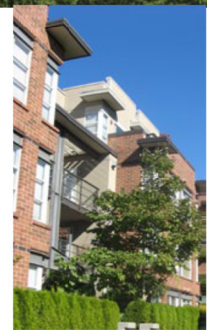


**Punched windows create variation in the façade and help create human scale**

## Materials

- n) An integrated, consistent range of materials and colours should be used, and variety between buildings and building frontages should be provided.
- o) In general, new buildings should incorporate substantial, natural building materials into their façade to avoid a 'thin veneer' look and feel. A robust and rugged, as opposed to skin like treatment, is preferred.
- p) Materials which are considered for use as an exterior finish should respect the nature and style of the original materials used on the facades of the prevailing historic buildings in downtown Aldergrove, such as brick, horizontal wood siding, and cut stone. Original, historic, building materials should be retained whenever possible during restorative renovations. Historic material should never be covered with modern materials. If historic materials have been covered over due to a previous renovation, they should be uncovered and refurbished to as near original condition as possible.
- q) Synthetic materials are generally discouraged in favour of natural materials such as masonry, stone and wood.
- r) The following materials are preferred for the downtown:

- Granite, marble, sandstone, facing stone laid in regular, coursed, patterns to imitate structural stone, and stone tiles such as granite and marble;
- Traditional brick;
- Glazed tile;
- Horizontal wood board siding utilizing traditional wood siding sizes, patterns, and methods of application such as wood plank, beveled wood, and clap board siding.



**Substantive natural materials including brick, wood and cut stone are preferred for residential and commercial buildings**



**Wood and brick combined with metal, glass, and limited stucco to create a pleasing palette of materials and colour.**



**Vinyl siding and large expanses of stucco are strongly discouraged and should be avoided**

- Wooden corner boards;
- Substantial wooden trims and moldings for windows and doors;
- Sawn wood shingles – both plain and fancy shapes (e.g. fish scale, diamond);
- Common brick in traditional sizes and colours and traditional textural techniques such as corbelling, and inset panels with half and full brick;
- Metal, formed into traditional decorative elements such as flashings, cornice, brackets, finials;
- Non-ferrous metals such as copper, brass, bronze, and zinc, used for decorative purposes; and
- Wood-like hardboard products (acceptable not preferable).

s) The following materials are strongly discouraged for the downtown:

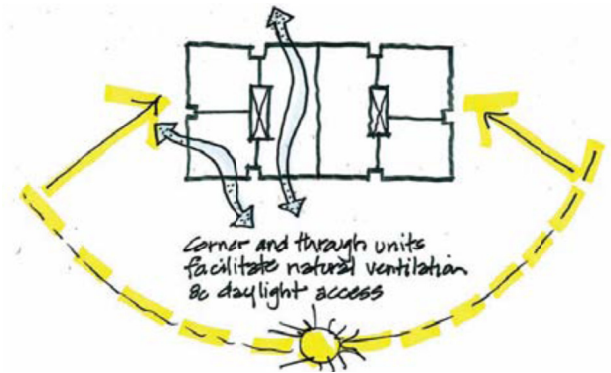
- Vinyl;
- Heavy swirl type stucco such as Bavarian style, and excessively bright coloured stucco;
- Random coursed 'ashlar' stone veneers and cobble stone;
- Modern oversized or undersized brick forms, modern pastel coloured bricks and artificial brick facing;
- Artificial brick or artificial stone;
- Asbestos or asphalt shingles or panels as a wall covering;
- Fiberglass panels;
- Corrugated coloured metal used as a siding material;
- Unfinished form cast concrete;
- Unfinished plywood, or chip board, used as a finish siding; and
- Vinyl for window frames.

### 3.10 Green and Healthy Buildings and Landscapes

**Intent:** To encourage building design and site planning that maximize livability, daylight access and energy efficiency and reduce the overall 'ecological footprint' (energy use, waste, and pollution) of development.

#### Guidelines

- Site and design new development to maximize the privacy of adjacent outdoor private open spaces.
- Site and orient new development so that a majority of primary living spaces receive direct sunlight for the daylight hours at Equinox.
- Residential buildings should be designed to receive daylight and natural ventilation from at least two sides of a building, or from one side and a roof. Where possible, dwellings should have a choice of aspect, either front and back, or on two sides for corner units.
- New buildings should be designed with greater floor to ceiling heights (i.e., 9' – 10') to increase the amount of interior space that can be day-lit from windows.
- Dwelling units with exterior access on only one side should always face a good view, the direction of the sun, or ideally both, and are more suitable as wide frontages with shallow floor plans to allow adequate penetration of daylight. Dwelling units with exterior access on two sides are usually suitable with narrow frontages and deep floor plates.
- New developments should ensure that the siting, form, and scale of buildings do not block significant views and solar access from existing or anticipated development, and that shadowing impacts on adjacent residential buildings and useable open spaces are minimized.
- Residential and mixed-use projects should incorporate courtyards and greenways as defining elements of the project while providing a common garden area, play space, gathering place, walkway, or other use located to maximize the amount of direct sunlight received.
- Incorporate safe and sunny play areas for children that have surveillance from ground oriented and upper storey dwellings in residential developments.

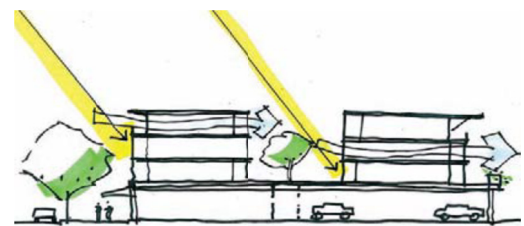


**Corner and through units facilitate natural ventilation and daylight access**



**Green roofs and upper storey step-backs provide amenity space for residents**

**Fixed fins shade the interior from afternoon sun, reducing summer cooling loads**



**Court yard designs create dual aspect units and amenity spaces for residents**



**A landscaped pathway located adjacent to this residential building partially screens the lower floor units from the park (foreground) while also softening the building's appearance, helping to integrate it into the landscape.**



**Storm water source controls like these rain gardens reduce pollutant run-off**

- i) Roof top common open spaces are encouraged, particularly where at-grade space is limited. Where possible, upper storey terraces are encouraged to open onto roof top gardens to increase access to semi-private outdoor amenity space.
- j) Incorporate green roofs where appropriate to help absorb storm water and provide outdoor amenity space for residents/workers.
- k) Best management practices for retention and infiltration of rain water shall be used as appropriate.
- l) Light pollution should be minimized through the use of full cut-off lighting, avoiding light reflectance. Exceptions may be made for signage and architectural lighting.
- m) Make use of existing buildings where possible or carefully deconstruct building and re-use materials.
- n) Use of materials with recycled content is strongly encouraged.
- o) Each dwelling unit in a residential or mixed-use project should incorporate direct access to a usable private outdoor space such as a patio, balcony, or upper level terrace. These should be of adequate size and be covered to ensure comfort and usability.
- p) New development should minimize the removal of existing significant trees. Where tree removal is necessary, trees that were removed should be replaced with new trees. All new developments are subject to the provisions of the Township of Langley Tree Protection Bylaw 2006 No. 4470.
- q) Landscaping should incorporate and emphasize native landscape materials and the use of drought resistant plants (xeriscaping) to reduce water usage from irrigation needs.
- r) New developments should incorporate a combination of landscaping materials to enhance and integrate new projects into the surrounding landscape and to improve the experience and overall livability of residents and users of new developments.
- s) The form of buildings should be softened using plants shrubs and trees, and where necessary, hard landscaping treatments such as terraced retaining walls and planters.

### 3.11 Public Realm Design

**Intent:** To ensure the design of streets and open spaces create visual interest, comfort and safety for pedestrians and contribute to a unique local identity and sense of place.

#### Guidelines

- a) Maintain a minimum distance of 3.5 m from curb to building face along all commercial streets in the downtown.
- b) Provide a continuous planting of street trees along both sides of streets and along greenways. Street trees should be directly planted into the ground using cutouts and ornamental grills in the sidewalk.
- c) Provide zebra or ladder painted crosswalks or crosswalks made of special paving materials at all key pedestrian crossings to increase driver awareness.
- d) Incorporate corner bulbs to enhance pedestrian crossings and provide space for landscaping, seating and public art, and raised crosswalks of special paving materials to calm traffic and emphasize key pedestrian crossings, with a priority for the pedestrian crossing at the Aldergrove Transit Plaza on 272 Street. Corner bulbs and raised crosswalks shall not be located along the Fraser Highway and 272 Street where curbside travel lanes shall remain open for peak period traffic flow and for bus travel in the curb lane.
- e) Provide adequate public streetscape amenities including benches, planters, trash cans, bike racks, water fountains, wayfinding signage, public telephones, and newspaper boxes, with a priority for the downtown Aldergrove transit plaza and other areas with high pedestrian activity. Furniture and fixtures should generally be located within a 1.5 m street furnishing zone located adjacent to the curb and should not impede pedestrian travel.
- f) Café tables are permitted and encouraged on public sidewalks provided safe passage for pedestrians and emergency services is maintained.
- g) Pedestrian-oriented lighting should be provided throughout downtown streets with priority for high pedestrian areas. These luminaries should be attached to existing light standards and incorporate cut-off shields to minimize light pollution.
- h) Distinctive bus shelters or deep canopies at building facades adjacent to the transit plaza should be provided for the comfort of transit users.



**Pedestrian scale lighting, special pavers along sidewalks and across intersections, curb extensions, lighted bollards, rolled curbs, street tree with pavers in the tree well, all combine to make a pleasing and welcoming streetscape for pedestrians**



**Street furniture can enhance the functionality and visual interest of downtown streets and plazas, and help create a unique identity and character for the downtown**

**Raised cross**

**special  
pavers**

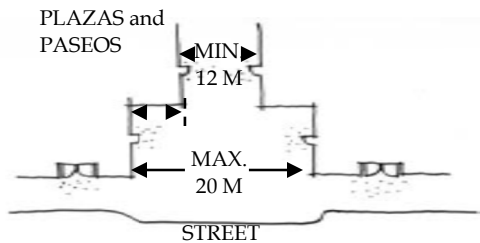
**Sidewalk/  
plaza cafes**



**public**

**Pedestrian  
lighting**

**continuous  
planting of  
street trees**



**Shared streets or woonerfs in appropriate  
locations can create a special character for  
certain streets in the downtown**



- i) Plazas and paseos (pass throughs) should be incorporated throughout the downtown at key locations as proposed by the concept plan to provide public 'outdoor' rooms and mid-block pedestrian connections.
- j) Plazas and greenways should be framed by buildings with a minimum building face separation of 12 m and a maximum building face separation of 20 m
- k) Paseos or mid-block pedestrian pass throughs should have a minimum building face separation of 8 m.
- l) Shared streets or woonerf's (streets that integrate vehicular, pedestrian and bicycle traffic together) are encouraged in active retail areas where appropriate, as indicated by the concept plan.

## **Public Art**

Public art can play an important role in reflecting Aldergrove's history and sense of place. It can be inspiring or playful, serious or whimsical, can be functional or make a statement – or all of these. It can be located to reflect a significant historic event, building, activity or natural feature. It can be in the form of sculptures, statues, murals, or a range of other forms, and should engage viewers of all ages to enhance the family-friendly tone of the downtown. Public art with touchable elements can draw active engagement transforming a street into a 'magical place'.

- m) Public art should be incorporated into parks, plazas and other key activity areas in the downtown, especially at gateways and entry points, as a key means of creating a unique identity and sense of place in the downtown and enhancing the sense of entry into the downtown.
- n) Public art in the downtown should reflect Aldergrove's unique natural and human history, for example, its agricultural roots and its local First Nations history.



**Public art can play  
an important role  
in reflecting  
Aldergrove's  
history and sense  
of place**

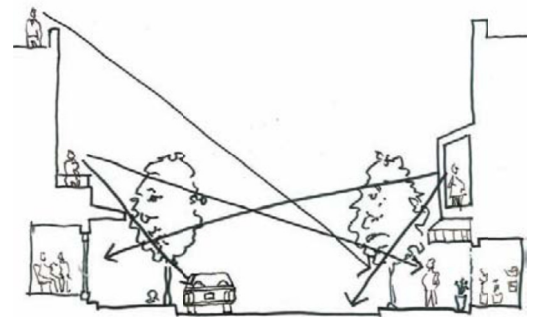


### 3.12 Personal Safety, Security and Accessibility

**Intent:** To ensure that building siting, orientation, and design enhance personal safety and security. To ensure buildings and open spaces accommodate and provide access for all users and abilities.

#### Guidelines

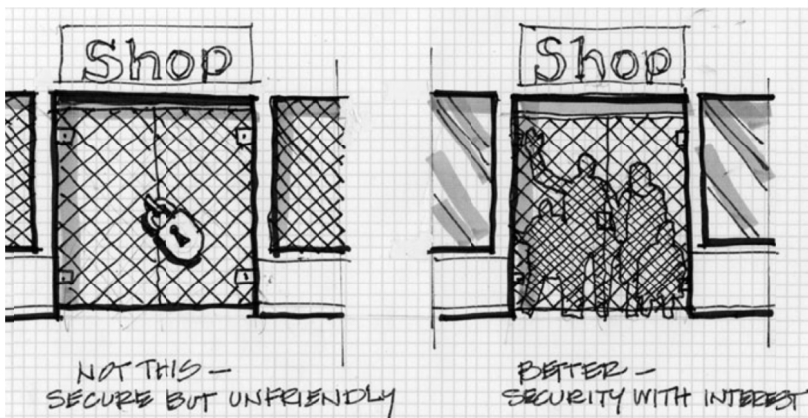
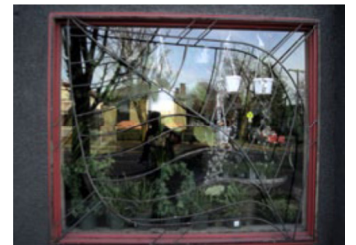
- a) Ensure the design of new development, through the placement of windows, balconies, and street-level uses, contributes to increased 'eyes on the street' and allows for casual surveillance of parks, open spaces, and children's play areas.
- b) Avoid blank, windowless walls that do not permit residents or workers to observe the street.
- c) Incorporate creative use of ornamental grills over ground floor windows or as fencing as necessary and where appropriate.
- d) Provide adequate lighting along streets and at entrances to enhance the sense of personal safety and security.
- e) Design parking areas to allow natural surveillance by retaining clear lines of sight to and between public sidewalks and building entrances both for those who park there and for users of nearby buildings.
- f) Eliminate structures and/or landscaping and plant materials that provide hiding places for undesirable activity. Generally, landscaping that is above the head or below the knee is appropriate.
- g) Ensure all pedestrian routes including those leading to building entrances are safe and easy to use by a wide range of persons with disabilities. Generally, such routes should be direct, level, obstacle free, easily identifiable and clearly separated from vehicular routes.

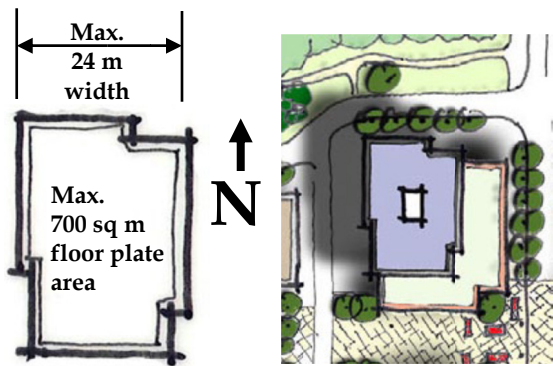


**Buildings should be designed and oriented to encourage casual surveillance and 'eyes on the street'**



**Ornamental grills provide security and visual interest**



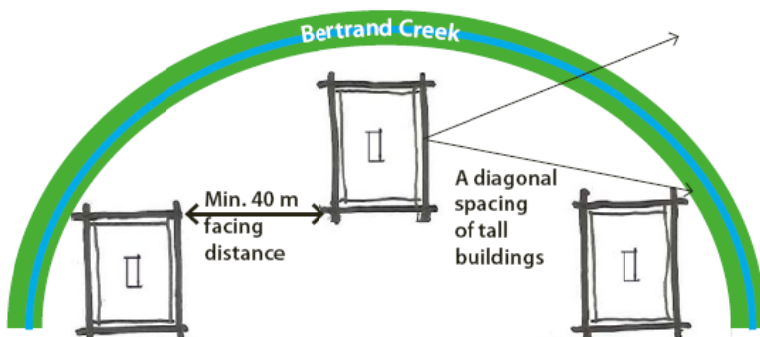
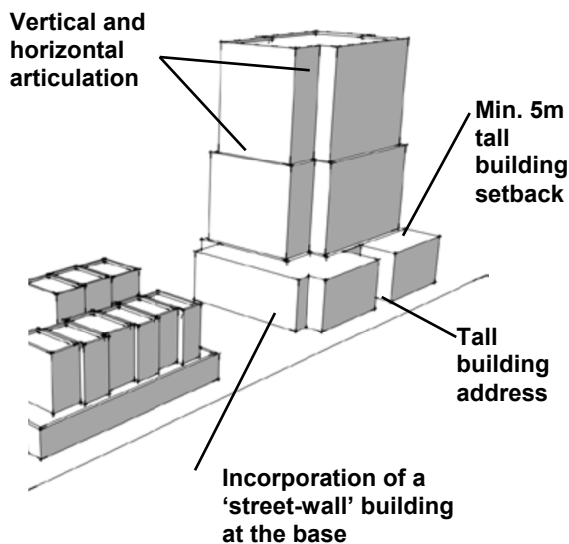


### 3.13 Tall Buildings

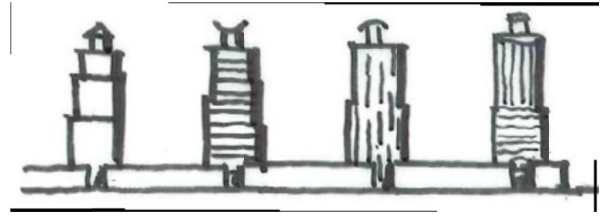
**Overview:** The overall intent of the tall building guidelines is to encourage siting, massing and design that minimize negative impacts on views, privacy, and solar access for individual units, to reduce the perceived bulk of tall buildings, and to minimize impacts of tall buildings on adjacent public streets and open spaces. Tall Buildings are defined as buildings over 6 storeys or 18 m in height. In addition to the preceding general guidelines (sections 3.1 – 3.12), the following tall building guidelines are applicable to development proposals that include buildings over 6 storeys in height.

#### Guidelines

- An open spacing of tall buildings should be maintained to ensure adequate light, air, access and views for residents.
- The minimum facing distance between tall buildings should be 40 m.
- The placement of tall buildings should achieve a diagonal spacing to avoid tall buildings looking directly into each other.
- Tall buildings should have a maximum floor plate size of 700 sq m.
- Tall buildings should have a maximum floor plate width of 24 m.
- The bulk of towers should be minimized using vertical and horizontal articulation, for example, by incorporating changes of plane, stepped terraces or modulated plan and façade forms.
- Tall buildings should have a maximum height of 50 m, excluding appurtenances and mechanical equipment.
- Tall buildings should generally be aligned parallel to the street in a north-south direction.
- New developments with tall buildings should incorporate a base building sited and scaled to complement adjacent buildings and to create a strong street edge definition. (See section Street Definition Guidelines, section 3.3).



- j) Tall buildings should incorporate ground floor uses that have views into and, where possible, access to, adjacent streets, parks and open spaces.
- k) Tall buildings should be setback a minimum of 5 m from the fronting public street or open space, while still achieving good address on the fronting public street or open space.
- l) Tall building address should be achieved by stepping back the base building (podium) at the primary entrance of tall buildings to allow the tall building to meet the street and by locating main building entrances so that they are clearly visible and directly accessible from the public sidewalk, plaza or other open space.
- m) An interesting and varied roof form should be achieved, for example, by incorporating a top penthouse or amenity space to conceal appurtenances and mechanical equipment.



**Variation in tower form and design should be achieved**

**An open spacing of tall buildings and their siting adjacent to Bertrand Creek minimize view, shadowing and other impacts on residents and pedestrians. Tall buildings front onto and have direct access from adjacent public streets and open spaces. Tall buildings incorporate a base building to define streets and open spaces**



### 3.14 Master Planning for Tall Buildings and Large Sites

In order to meet the objectives of downtown revitalization as indicated in the Aldergrove Core Area Plan and illustrated in the Aldergrove Core Area Concept Plan, proposals for Large Sites and sites with Tall Buildings should include a master plan. The integration of larger sites and sites with tall buildings with adjacent areas is important, especially in light of Area Plan policies calling for the provision of new roads, parks, pedestrian paths, plazas and open spaces that extend into and complement the existing surrounding area. Further, master planning will enable tall buildings to be sited and organized in a way that provides desirable transition to adjacent areas and ensures appropriate tall building separation. More broadly, a master planning process for these sites will help knit the public realm into a single, cohesive whole as demonstrated by the illustrative concept plan and encouraged by the design guidelines.

A Master Plan is to be provided at both the neighbourhood scale and the site or block scale and should reflect the intent of the Core Area Concept Plan and Design Guidelines.

Tall Buildings are defined as buildings over 6 storeys or 18 m in height. Large Sites are defined as those over 5000 sq. m in size. However, the Township may, at its discretion, identify other sites with special characteristics or conditions where master plans will be required. A Master Plan for Large Sites and site with Tall Buildings should describe in drawings and words for the site and its context the following issues:

- the location and dimensions of public streets, parks and accessible open spaces
- general location and dimensions of pedestrian circulation and relationship to pedestrian sidewalks and paths, transit stops and shelters
- general location of building footprints – base buildings and taller buildings
- general layout and dimensions of setbacks from streets, parks and open spaces, as well as dimensions between base and tall buildings on the same site
- general location of building entrances for each building
- general location and dimensions of site access, service areas, ramps, drop-off and parking for each building
- the location of watercourses including non-disturbance areas
- phasing plan and schedule
- perspective(s) showing important views
- shadowing impacts on adjacent buildings and open spaces using sun/shade diagrams at the following times:
  - Equinox: 8am, 12 noon, 4pm
  - Winter Solstice: 9am, 12 noon, 3pm



Township of Langley  
20338 - 65 Avenue  
Langley, British Columbia V2Y 3J1

Community Development Division  
604.533.6034

[tol.ca](http://tol.ca)